

# CITY OF DINUBA GENERAL PLAN POLICIES STATEMENT



Adopted September 23, 2008

# CITY OF DINUBA

## GENERAL PLAN POLICIES STATEMENT



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Adopted September 23, 2008

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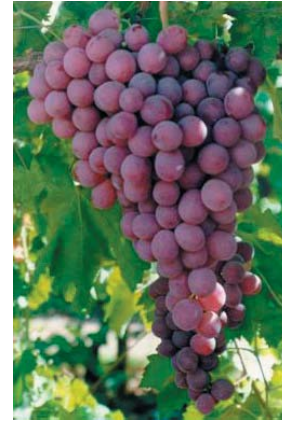
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**INTRODUCTION**

California state law requires each city and county to adopt a General Plan “for the physical development of the county or city, and any land outside its boundaries which bears relation to its planning” (Government Code §65300). The California Supreme Court has called the General Plan the “constitution for future development.” Dinuba’s General Plan expresses the community’s development goals and embodies public policy relative to the distribution of future land uses, both public and private. State law specifies that each General Plan address seven issue areas (“elements”): land use, circulation, open space, conservation, housing, safety and noise. Additional elements may be added as a local option.



The Dinuba General Plan provides comprehensive planning for the future. It encompasses what the City is now, what it intends to be, and provides the overall framework of how to achieve this future condition. Estimates are made about future population, household types, and employment, so that plans for land use, circulation and public facilities can be made to meet future needs. The General Plan represents an agreement on the fundamental values and a vision that is shared by the residents and the business community of Dinuba and the surrounding area of interest. Its purpose is to provide decision makers and City staff with direction for confronting present issues, as an aid in coordinating planning issues with other governmental agencies, and for navigating the future.

- The Land Use Element provides the central policy context on which to base all land use decision making in Dinuba. It is through the implementation of the goals, objectives, and policies, that the future land use pattern of Dinuba will continue to be shaped.
- The Housing Element looks at the current and future need for housing units, the capacity in the City for additional units, the types of households that will need some form of assistance or special housing, and ways to perpetuate existing housing. Dinuba’s Housing Element was adopted in late 2004 and is in compliance with the Department of Housing and Community Development.
- Transportation routes, design standards for streets and neighborhoods, and current and future traffic levels on City streets are among the issues covered in the Circulation Element of the General Plan.
- Conservation issues include strategies for an orderly transition from agriculture to urban uses, groundwater recharge, conservation of ground water

resources, and commitment to conservation of agricultural lands and the City’s greenbelt.

- Open space and recreation issues include discussion of parks and recreation resources, targeted growth of these facilities, the creation of a city-wide bike/pedestrian path system and targeting open space to function in a multi-use capacity.
- Existing and future noise from traffic, rail and other activities are issues discussed in the Noise Element.
- The Safety Element of the General Plan analyzes conditions in the City and surrounding area that may be hazardous to those who live and work there, such as flood inundation, fire and hazardous materials.

Each of these issue areas have goals, objectives, and policies designed to provide a safe and pleasant environment in the future. Dinuba’s General Plan contains not only the seven mandatory elements required by state law, but also several optional elements. These include: Public Services and Facilities, Urban Design, and Urban Boundary. Each General Plan chapter covers an aspect of the City’s growth and development. Components of each section are interrelated and therefore must be consistent with each other. Taken together, they provide the guidance for all aspects of planning for the future.

Having adopted the General Plan, the City assumes the responsibility to implement it, to report on its continuous status, and to communicate with citizens and other agencies regarding the Plan’s policies.

**ORGANIZATION OF THE GENERAL PLAN**

This General Plan is an update, expansion and reorganization of the 1997 General Plan. Significant changes to the 1997 General Plan have occurred; including expanding the boundaries of the Sphere of Influence (SOI) and Urban Development Boundaries (UDB), an updated circulation system; and a new Planning Area beyond the SOI. The Planning Area encompasses the City limits and SOI, and unincorporated territory bearing a relation to the City’s planning.

The Dinuba General Plan consists of three separate documents – a General Plan Background Report, the General Plan Policies Statement, and the Environmental Impact Report (EIR) and technical appendices. The General Plan Policies document contains nine (9) chapters. The focus of each element consists of goals, objectives and policies associated with the major issue areas. Some of the elements contain related background information required by State law. The EIR presents three alternatives and documents how the proposed plan will impact the environment as compared to the alternative plans. The technical

*The seven mandatory elements of any General Plan are:*

- Land Use
- Circulation
- Housing
- Conservation
- Open Space
- Noise
- Safety

*A goal is a general direction-setter. An objective is a specified end, condition, or state that is an intermediate step toward attaining a goal. A policy is a specific statement that guides decision-making.*

appendices contain technical reports and background information (i.e., noise, traffic, etc.) which provide a more detailed analysis.

## CONTEXT

The Dinuba General Plan Planning Area is located within northern Tulare County, approximately 15 miles north of Visalia and 25 miles southeast of Fresno. The Dinuba General Plan covers an approximately 22 square mile Planning Area which encompasses the City of Dinuba and unincorporated Tulare County, inclusive of the City's SOI. All lands outside of the City's boundary are regulated by the Tulare County General Plan and zoning designations. However, State law, however, requires that cities plan for areas outside of their immediate jurisdiction, if the areas have a direct relationship to planning needs. The SOI also establishes the area in which annexations may be permitted.

The Dinuba Planning Area consists of three distinct geographical areas: The City, which represents the incorporated City within the City limit boundaries; the SOI, which is slightly larger than the City's previous SOI; and the Planning Area, which includes unincorporated Tulare County lands outside of the City's SOI.

## INTENT OF THE PLAN

This General Plan was developed through a cooperative effort involving the City Council and Planning Commission, City staff and their consultants, a General Plan Advisory Committee (GPAC), and interested citizens who participated in multiple "visioning workshops." The General Plan Update process disclosed a number of important issues that have been instrumental in shaping the Plan. Some of these issues are as follows:

- Continuation and expansion of the City's greenbelt;
- More walkable, neighborhood oriented subdivisions;
- Dinuba needs to maintain its "small town" atmosphere;
- More mix of uses needed in the downtown;
- More recreational opportunities needed;
- More upscale/above moderate housing needed;
- Creation of a City-wide bike/pedestrian path system;
- More aesthetic streetscape needed along El Monte and Alta;
- More variety in housing (recessed/detached garages, smaller setbacks, mix of housing);
- Contiguous growth in all directions, but primarily to the west;
- New industrial to be an expansion of existing industrial area; and
- New hospital, fire station and high school needed on west side of town

The Planning Commission and City Council adopted Planning Principles on October 26, 2005 which helped to guide the development of the General Plan and asked that a General Plan Advisory Committee review the policies appro-

priate to implement these principles and to develop a land use plan. The Planning Principles are as follows:

- Dinuba's friendly, small town atmosphere should be preserved by ensuring diverse, appropriate scale retail services; citizen participation in decision making; adequacy of public services; and quality of public schools;
- New residential development should be more walkable, have an inviting neighborhood look and feel, and include some of the following: narrow, tree-lined streets; an alternative to sound walls; recessed/detached garages; include ample open space/parkland; and a mix of housing types;
- The major gateways into Dinuba should be aesthetic and inviting to both travelers and residents. This shall be accomplished with appropriate signage and landscaping at appropriate locations;
- New development should take place in a concentric pattern, contiguous to existing developed areas;
- The City's Sphere of Influence and growth policies should ensure that the community is physically distinct from others and contains an agricultural buffer area;
- Traffic conflicts should be resolved, including connectivity between neighborhoods, critical intersections and access to industrial areas. Growth should be allocated with accessibility constraints in mind;
- Local and minor collector streets should be used to provide connectivity between neighborhoods while limiting cross-town trips through neighborhoods. Collector and arterial roads should be designed to provide efficient, safe connectivity between neighborhoods, services and facilities;
- Opportunities to provide more public spaces for recreation and social events should be capitalized on;
- The role of the downtown area as a focal point of the community should be preserved. Sites for more retail, entertainment, restaurants and specialty stores should be identified; and
- The commercial opportunities afforded by Alta Avenue and El Monte Way should be capitalized on.

## ADMINISTERING THE GENERAL PLAN PROGRAM

Once adopted, the General Plan does not remain static. State law provides direction on how cities can maintain the plan as a contemporary policy guide. Government Code §65400 [b] directs the Community Development Department to report annually to the City Council on the status of the Plan and progress in its implementation.

Over time it may be necessary to re-evaluate the goals, objectives and policies and modify them due to changes in the environment, regional considerations, and the economy. Up to four general plan amendments per year for each mandatory element are permitted by State law. It is required that any decision on a general plan amendment be supported by findings of fact.

## GENERAL PLAN REQUIREMENTS

While they allow considerable flexibility, state planning laws do establish some requirements for the issues that general plans must address. The California Government Code establishes both the content of general plans and rules for their adoption and subsequent amendment. Together, state law and judicial decisions establish three overall guidelines for general plans.

- **The General Plan must be comprehensive.** This requirement has two aspects. First, the General Plan must be geographically comprehensive. That is, it must apply throughout the entire incorporated area and it should include other areas that the City determines are relevant to its planning. Second, the General Plan must address the full range of issues that affects the City's physical development.
- **The General Plan must be internally consistent.** This requirement means that the General Plan must fully integrate its separate parts and relate them to each other without conflict. The internal consistency requirement has five dimensions: equal status among elements, consistency between elements, consistency within elements, area plan consistency and text and diagram consistency.
- **The General Plan must be long-range.** Since the General Plan affects the welfare of current and future generations, state law requires that the plan take a long-term perspective (§65300). The General Plan projects conditions and needs into the future as a basis for determining objectives. It also establishes long-term policies for day-to-day decision-making based upon those objectives.



## 1.0 LAND USE ELEMENT

### INTRODUCTION

The Land Use Element is a guide to future land use within Dinuba and affects many of the issues addressed in the other General Plan Elements. The Land Use Element identifies the type and location of future land uses within the City. The specific land uses and their location within the community in turn affect the remaining General Plan Elements. For example, the location and type of land uses outlined in the Land Use Element affect the circulation system that is identified in the Circulation Element, and the land uses identified in the Land Use Element also reflect the community's goals for its future form and character, as outlined in the Urban Design Element. In addition to land uses, the Land Use Element also addresses how growth will occur, with special attention given to public services and facilities and economic development.

### PURPOSE OF THE LAND USE ELEMENT

As a city, state law requires that Dinuba prepare and adopt a General Plan as a tool to manage growth and development. The Land Use Element is one of the seven mandatory Elements of the General Plan.

The purpose of the Land Use Element is to describe present and planned land uses and their relationship to the community's long-range goals for the future. The Land Use Element identifies the proposed general distribution, location, and extent of land uses such as residential, commercial, industrial, and public/quasi public. The Element consists of text and a map (reference map pocket) that outline the future land uses within the City and how these uses are integrated with the other General Plan Elements and policies. The Land Use Map is a particularly important feature of the Element since it shows the location and types of development within the City. The Element also describes the intensity or density of development planned for the community. The general location of future growth is also defined in the Element.

The Land Use Element of the Dinuba General Plan represents the City's desire for long-range changes and enhancements of land uses. Finally, the goals, objectives and policies contained in this Element establish the framework for future land use planning and decision making in Dinuba.

### SCOPE AND CONTENT OF THE LAND USE ELEMENT

The Land Use Element complies with the requirements of the General Plan Land Use Element mandated in Government Code §65302(a). The Element is comprised of five sections: the Introduction; Purpose of the Land Use Element; Scope and Content of the Land Use Element; Goals, Objectives, and Policies and Standards; and Land Use Map. In the Goals, Objectives, and Policies section, major land use issues are identified and related goals and poli-

cies are established to address these issues. The goals, which are overall statements of community desires, are comprised of broad statements of purpose and direction. Policies serve as guides for reviewing development proposals, planning facilities to accommodate anticipated growth, and accomplishing community economic development strategies. To achieve the goals, objectives and policies, a logical, organized land use pattern is established with standards for future community development. The Land Use Map graphically identifies the planned land uses within Dinuba.

### GOAL 1:

Preserve and enhance Dinuba's unique character and achieve an optimal balance of residential, commercial, industrial, public and open space land uses.

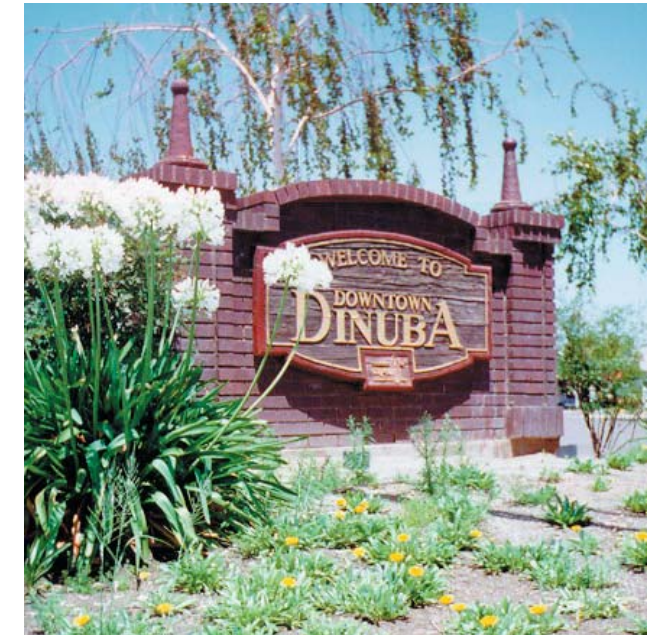
### 1.1 COMMUNITY IDENTITY

#### OBJECTIVES

- A. Strive to keep Dinuba separate and distinct from nearby communities.
- B. Maintain and enhance Dinuba's physical diversity, visual qualities and small-town characteristics.
- C. Maintain the downtown core area (Tulare Street from "H" Street to "M" Street) as the City's geographic and social center.

#### POLICIES AND STANDARDS

- 1.1 Develop design review standards for structures, landscaping and related development to facilitate compatibility with surrounding uses and the overall character of the community.
- 1.2 Gateways to Dinuba and the downtown core area shall be designated and identified by well-designed, landscaped entrances to enhance access corridors. Land uses in the vicinity of Gateways should be of high quality design which reflects favorably on the image of the community.



*Gateways define the primary entrances to the city and provide a sense of arrival, reaffirm direction, and reinforce the identity and character of Dinuba. Gateway elements can include monumentation, fencing, orchard planting, seasonal plants and public art.*

- 1.3 Emphasize pedestrian amenities in the downtown area including landscaped open space areas or plazas, street furniture, lighting and signage.
- 1.4 Develop a City-wide street tree and landscape master plan to delineate neighborhoods, master and specific plan areas.
- 1.5 Develop scenic entryways (gateways) and roadway corridors into the City through special setback and landscape standards, entry signage, open space and park development, and/or land use designations. These corridors shall also have enhanced landscape standards. Gateways and entryways to be considered should include:

Gateways:

North:	South:
Alta Avenue/Avenue 430	Alta Avenue/Avenue 400 Alignment
East:	West:
El Monte Way/Road 96	El Monte Way/Road 54 Alignment

- 1.6 The City shall coordinate with Tulare County, Fresno County and the City of Reedley to develop a Specific Plan for the area south of the Fresno County line. The purpose of this Specific Plan is to identify and implement policies which will maintain a significant buffer zone between Dinuba and the City of Reedley, while allowing the affected property owners to realize a return on their investment. This will be accomplished through the purchase of agricultural or open space easements, in addition to Land Use and Zoning regulations.

**1.2 RESIDENTIAL LAND USE**

**OBJECTIVE**

Designate and allow for the development of a wide range of residential housing types in the City to meet the needs of all of the City's citizens.

**POLICIES AND STANDARDS**

- 1.7 Establish the following residential density designations:
  - a. Low Density Residential (0 - 2 dwelling units/gross acre). The low density residential designation is intended for estate residential development characterized by larger single family residential lots one-half acre in size or larger. The average density for this designation is 1-2 dwelling units per acre. All low density residential development shall be served by City sewer and water services. This land use shall be used on the Community's permanent edges such as roadways, waterways, or other physical feature types of standards where a full range of urban services

- may not be available and to areas where lower densities are required to conform with public safety or environmental constraints. Densities in excess of 1.5 dwelling units per acre shall have full urban improvements, shall not have farm animals, and shall require a Conditional Use Permit.
- b. Medium-Low Density Residential (2.1 - 4.5 units/gross acre). The medium-low density residential land use category provides for a land use pattern characterized by single family residential development with lot sizes larger than those within medium density. The usual development pattern found in such areas is a typical subdivision development with lot sizes generally between 8,500-12,500 square feet. This land use is most appropriately used when "Estate Residential" developments are desired and where the overall density of an area should be limited because of public facility or safety constraints.
- c. Medium Density Residential (4.6 – 7.5 units/gross acre). The medium density residential category provides for a land use pattern of predominantly single family development as permitted in the R-1 district. This designation also provides for innovative designs which utilize clustering, duplexes or half-plexes on corner lots, zero lot line, or planned development features. Lot sizes generally range from 4,500 – 7,000 square feet. Developments in excess of 7.0 units per acre should be encouraged for infill parcels, in specific plan or master plan areas, and where it will address unmet housing needs.
- d. Medium-High Density Residential (7.6 - 15.0 units/gross acre). This land use category provides for a land use pattern characterized predominantly by small scale multiple family residential developments. The typical residential pattern includes duplexes and larger scale, high-amenity apartments. Areas designated medium-high density residential are to be integrated throughout the community adjacent to transportation, community services and commercial developments. To avoid inappropriate concentration of these facilities, such developments shall be limited to 25 contiguous units when integrated into a single family neighborhood and to 50 contiguous units when developed as a free standing development. New development shall conform to the Urban Design Element of the General Plan.
- e. High Density Residential (15.1 - 24.0 units/gross acre). The high density residential land use category provides for the highest residential densities permitted in the City. It is intended that this category utilize innovative site planning, provide on-site recreational amenities, and be located near major community facilities, business centers, and streets of at least collector capacity. Projects in excess of 25 units or with a density in excess of 24 units per gross acre shall require a Conditional Use Permit. High density residential developments shall use high

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*Infill development is development that occurs on vacant or under utilized land within areas that are already largely developed.*



quality architectural design features, intensified landscaping, adequate open space, adequate parking, and adequate on-site recreational facilities. Such developments should be limited in size to no more than 50 units on one site to reduce the impact of such facilities on any one neighborhood in the community.

- 1.8 Each residential category indicates a range of density deemed reasonable and desirable for areas within the City. The maximum density indicated defines the number of units per gross acre within a given area. Residential development must provide at least the minimum number of units per gross acre indicated in the General Plan. This requirement is intended to encourage the location of certain residential product types and densities consistent with adjacent land uses, access, public services, and environmental concerns.
- 1.9 The City shall use the planned unit development and density bonus ordinances to provide density increases in accordance with State Law.
- 1.10 Manufactured and modular housing developments shall be permitted subject to design regulations and existing ordinances.
- 1.11 Second floor housing may be permitted by Conditional Use Permit in the Central Commercial designation as part of a mixed-use concept. Housing uses shall be discouraged on the ground floor of commercially designated properties.

**OBJECTIVES**

- A. Promote stable high quality residential neighborhoods.
- B. Encourage new residential neighborhoods that have the desirable characteristics of traditional small town neighborhoods.

**POLICIES AND STANDARDS**

- 1.12 New residential development abutting an arterial or collector will be encouraged to use a berm/swale with landscaping instead of a masonry wall between the landscaping strip, sidewalk and maintenance district setback. A masonry wall may be used behind the berm/swale or the berm/swale may go half-



way up the masonry wall. This will limit the view of the wall from the street and still provide privacy to residents.

- 1.13 The General Plan map has identified areas where the majority of new growth will occur. Prior to approval of any development applications within these areas, the following actions must take place:
  - a. The City will Master Plan each new square mile growth area, which will guide future development. The Master Plan shall, at minimum, identify land uses and densities, road layout, public facilities requirements and possible school locations. The Master Plan shall also contain design guidelines for all land use types within the Plan area.
  - b. Following the adoption of the Master Plan, new development proposals shall submit a Specific Plan application. The Specific Plan shall conform to the requirements of the Master Plan, as well as the City's General Plan goals and policies. Projects less than 10 acres in size may request a waiver from the City Council, providing the Council can make the finding that the project is in substantial conformance with the Master Plan.
- 1.14 Multifamily residential developments with more than 25 units shall have direct access to a collector or arterial street, and where feasible, be located near commercial and community services.
- 1.15 Development standards for the interface between multifamily residential and single family residential shall be as follows:
  - a. Outdoor recreational areas, game courts, pools, and solid waste collection areas on multifamily properties shall be oriented away from adjacent properties planned for single family residential.
  - b. Multifamily parking areas, garages, other structures, and access drives shall be separated from adjacent properties planned for single family residential with a 10-foot landscaped setback containing deciduous and evergreen trees.
  - c. Exterior area lighting for multifamily residential parking, carports, garages, access drives, and other recreation areas, shall be shielded to prevent line of sight visibility of the light source from abutting property planned for single family residential.
  - d. Multifamily buildings greater than 15 feet in height shall be prohibited within 25 feet of abutting property planned for single family residential. An additional 10 feet of setback shall be required for each additional story.

Manufactured homes are residential structures that are constructed entirely in the factory, and that since June 15, 1976, have been regulated by the federal Manufactured Home Construction and Safety Standards Act of 1974 under the administration of HUD.

- 1.16 Where new residential development is proposed that adjoins existing commercial or industrial development, the residential developer shall be required to provide an architectural transition. This transition may include such provisions as building setbacks, landscaping and masonry wall requirements to benefit future residents.
- 1.17 In order to encourage infill development, flexible design standards should be developed which meet the intent of the General Plan.
- 1.18 Multifamily developments shall use intense landscaping. Block walls adjacent to the public right of way shall be discouraged unless they are found to be necessary for public health and safety.
- 1.19 Neighborhoods should be designed, with emphasis placed on high-quality construction and innovative architecture, to provide a “sense of place” and preserve the City’s small-town character while offering a choice of residential densities and costs that meets the varying needs of residents.
- 1.20 New single family houses, duplexes and townhouses will be encouraged to include front porches in their design.
- 1.21 Garages for new single family houses, duplexes and townhouses should be subordinate in visual importance to the house itself, especially the entry. This should be achieved by encouraging the location of garages toward the back of properties, encouraging detached garages, requiring garages to be set back from the front edge of the house and encouraging the orientation of garage doors 90 degrees from the street.
- 1.22 Neighborhoods should be physically connected to one another via a series of Minor Collector roadways and pedestrian paths, and all residents should be within a short walk or drive of retail and other services. New development shall coordinate with the irrigation districts regarding the usage of district facility corridors as walking/bicycle paths available for public use.
- 1.23 Planting strips will be encouraged on all residential streets with sidewalks a sufficient width to allow for street trees between the curb and the sidewalk.
- 1.24 Commercial uses may be located either in the center or at the periphery of neighborhoods, and should be integrated with residential uses and designed to be as accessible and appealing to pedestrians as possible, in order to encourage walking and biking.

### 1.3 COMMERCIAL LAND USE

#### OBJECTIVE

- A. Ensure the provision of adequate commercial shopping opportunities and office space locations to meet anticipated needs.
- B. Enhance the viability of the downtown and preserve its role as the heart of the community.

#### POLICIES AND STANDARDS

- 1.25 Establish the following commercial land use designations:
  - a. Neighborhood Commercial. The neighborhood commercial land use designation provides for a 1-5 acre cluster of commercial establishments serving the everyday convenience goods and personal service needs of a defined neighborhood. The service radius of a neighborhood commercial use is generally 1/2 mile.
  - b. Community Commercial. The community commercial land use designation provides for a 10-acre or larger cluster of commercial establishments serving needs similar to the neighborhood commercial centers, but also includes grocery, drug, general merchandise, variety, and specialty stores. The community commercial center generally serves a market area of 1-2 miles. Such facilities should be located in each residential quadrant of the community to minimize cross-town traffic.
  - c. Central Commercial. This designation provides the City with a mixed use activity center oriented towards the downtown area.
  - d. General Commercial. This designation provides for commercial areas with a wide range of retail and service activities along major traffic corridors, such as El Monte and Alta.
  - e. Office Commercial. This designation provides for office development which includes medical, dental, law, or other professional offices. Commercial uses contemplated as part of this category include business support services and support restaurant and medical services. High density residential uses are also allowed in the office commercial designation subject to a Conditional Use Permit.
- 1.26 Neighborhood and Community Commercial sites should be located at or near the intersection of collector and/or arterial streets with a minimum of overlap with other existing or planned Neighborhood or Community



- Commercial uses. Only one neighborhood commercial development may be permitted at any one intersection. Such developments should also be directly accessible from adjacent residential developments to encourage walking and biking.
- 1.27 Community Commercial uses should be located along major traffic ways in consolidated centers that utilize common access and parking for commercial uses, discourage the introduction of strip commercial uses, and require adequate pedestrian links to residential areas.
  - 1.28 The Central Commercial designation should be used in the downtown area in order to attract and accommodate growth which includes commercial, financial, office, entertainment, governmental and limited residential uses.
    - a. Rehabilitation of existing structures to accommodate residential and office facilities in the upper floors in the Central Commercial district should be encouraged.
    - b. Live/work units, in which the unit is both a place to live and a place of business, are allowed in the Central Commercial designation as long as the place of residence is in a separate room from the place of business.
  - 1.29 The General Commercial designation should be applied along arterial streets to provide commercial support for nearby Community and Central Commercial uses as well as industrial areas. General Commercial includes freestanding uses which do not fit well in unified centers as well as service and highway commercial uses.
  - 1.30 Commercial Office land use unified designations which allow construction of new office unified centers, the redevelopment of existing areas to office use, and the conversion of older homes to offices along major streets. Where homes are converted to offices, the area should be a logical extension of existing or planned office/commercial uses, and the lot should be of adequate size to accommodate parking in the rear of the structure or outside of the front yard landscape setback. An office conversion zone should be developed to establish standards for conversion of existing residential structures. Office uses should be located in and adjacent to the downtown and near the hospital and other major medical facilities. Small scale office use should be permitted in the Community, Neighborhood and General Commercial districts. Access shall be improved to support proposed development. Development shall be compatible with adjoining residential neighborhoods.
  - 1.31 Commercial and office site planning shall be compatible with the surrounding neighborhood, signage, and landscaping.

- 1.32 The City will encourage the eventual phasing out of existing industrial uses, and detached single family homes and other uses surrounding the downtown that generally are not compatible with or supportive of a downtown commercial core area and their replacement with appropriate commercial, mixed use and high quality higher density residential uses.



- 1.33 The City will encourage the development of mixed use developments along Tulare Avenue, with residential and commercial uses in the same building.
- 1.34 New commercial development 5 acres or larger shall provide a minimum of 1% of the gross acreage to public space. Such space shall be developed as park/open space, art/sculpture, fountains, or other such amenities. All public spaces shall contain seating.

**OBJECTIVE**

Provide for the compatible integration of residential and commercial/office uses.

**POLICIES AND STANDARDS**

- 1.35 Development standards for the interface between commercial or office uses and residential uses shall be as follows:
  - a. A landscaped setback of at least ten feet wide containing deciduous and evergreen trees shall be planted and maintained along the property line between commercial and office uses and residential properties that have a common property line.
  - b. A masonry wall six feet in height, shall be erected along the property line where commercial and office uses have a common property line with residentially designated properties.
  - c. A masonry wall three and one-half feet in height, shall be erected along the setback line ten feet from the parallel with local streets abutting planned residential uses.



- d. All commercial loading and storage areas shall be screened from view of adjoining residential property by a combination of landscape planting and a masonry wall. Loading areas shall be enclosed and be located so that there are no noise impacts to adjacent residential properties. All storage shall be within an enclosed structure.
  - e. Roof-mounted and detached mechanical equipment shall be acoustically baffled to prevent noise from the equipment from exceeding 55 dB(A) measured at the nearest residential property line.
- 1.36 In order to encourage the integration of neighborhood and community commercial uses into neighborhoods, designs should de-emphasize the usage of walls as buffers where they create barriers to pedestrian access. Continuous block walls shall be discouraged, and offsets and openings shall be encouraged. Other types of uses, such as open space, may be utilized as buffers.

**1.4 GENERAL PLAN, ZONING CONSISTENCY AND PLAN ADMINISTRATION**

**OBJECTIVE**

Establish a well-balanced mix of residential, commercial, industrial, and open space/public land uses which will create and maintain a high quality environment and a fiscally sound community.

**POLICIES AND STANDARDS**

- 1.37 Land use density and intensity standards are shown in Table 1-1.
- 1.38 Zoning shall be consistent with the General Plan. A zone district shall be deemed consistent with a land use designation when such zone district is specified as consistent in the Plan Consistency Table. In no case, however, shall the overall maximum density of the plan designation be exceeded.
  - a. Residential density on part of a site may exceed the maximum if the entire project site density conforms with the Plan Consistency Table. Mixed residential uses and density incentives should be provided to most fully utilize properties. Such projects shall be at least two acres in size and will require a Conditional Use Permit.
- 1.39 When a General Plan Map amendment is required, the amendment and consistent rezoning application shall be processed concurrently. The City may require a Precise Plan Zone if it determines that such zone is necessary to protect adjacent land uses from impacts of the proposed use.

**Table 1-1  
Plan Consistency Table**

Plan Designation	Existing Consistent Zone District	Consistent Density (in dwelling units per gross acre)	Intensity Person/Acre
<b>Residential</b>			
Low Density	A-N, R-A	0.00-2.00	8
Medium-Low Density	R-1	2.10-4.50	15
Medium Density	R-1, RM-3	4.60-7.50	35
Medium-High Density	RM-2, RM-3	7.60-15.00	60
High Density	RM-2, RM-1.5, C-2	15.10-24.00	100
<b>Commercial</b>			
Neighborhood	C-1		
Community	C-2		
Central	C-2, RM-1.5, RM-2		
General	C-3, C-H		
Office	P-0, RM-3, P		
<b>Industrial</b>			
Light	M-1, M-1-X		
Heavy	M-2		
<b>Open/Public</b>			
Agriculture	A-N		
Parks & Open Space	R-1		
Public Buildings & Grounds	R-1		
Urban Reserve	A-N		

- 1.40 The City will update the Zoning Ordinance as appropriate to implement the General Plan.
- 1.41 Development standards shall be implemented for Alta and El Monte to improve the practical function and aesthetic quality.
- 1.42 The Conditional Use Permit process shall be used where site conditions or project location will affect land use compatibility. Findings required for approval shall include:
  - a. That the site for the proposed use is adequate in size and shape to accommodate said use and all yards, spaces, walls and fences, parking, loading, landscaping, and other features required by the applicable zone district.
  - b. That the site for the proposed use is served by streets and highways adequate to carry the quantity and kind of traffic generated by the proposed use.

- c. That public facilities are currently, or will be, adequate to serve the proposed use.
- d. That the proposed development is consistent with the General Plan.

- 1.43 The Urban Reserve classification denotes lands not anticipated for development within the 20-year time frame of the General Plan but which would be expected to ultimately urbanize. Lands within the Urban Reserve designation may be annexed for purposes of planning long-term urban service extensions. They may not be developed, however, without first amending the General Plan and after a need is demonstrated for development in these areas, and that urban services can be provided without adversely affecting the development feasibility of lands currently planned and zoned. The greenbelt shall be maintained on the northerly and easterly edges of the community.
- 1.44 The City should undertake a review of the General Plan’s demographic, financial, land use demand and supply, and infrastructure assumptions no less frequently than once every five years to provide an opportunity for necessary mid-term modifications to the General Plan. This review should include public participation.
- 1.45 Prior to annexation, specific plans and master plans should be utilized, where appropriate, to implement the General Plan.
- 1.46 Financing mechanisms for the development and maintenance of private and public improvements should be established to ensure that necessary infrastructure and public facilities are provided and that adequate provision is made for their ongoing maintenance and operation.

**1.5 PUBLIC AND INSTITUTIONAL LAND USE**

**OBJECTIVE**

Provide sites for adequate public facilities to serve projected growth.

**POLICIES AND STANDARDS**

- 1.47 Update the water, wastewater and storm drainage master plans, and any other specific or master plans related to infrastructure development on a periodic basis.



- 1.48 Annually monitor the need for law enforcement, fire and other emergency services personnel as the City grows.
- 1.49 Continue to plan and provide efficient public safety and leisure/cultural facilities and services for the community.
- 1.50 In considering development proposals which have the potential to affect school capacity, the City shall refer such proposals to the School District for review and comment to ensure that adequate school facilities are implemented to serve the proposed developments. Developers should mitigate impacts to schools in accordance with School District plans.

**OBJECTIVES**

- A. The City shall coordinate the location of school sites in the community with the school district in an effort to assist the School District in providing school facilities at the optimum locations and in a timely manner.
- B. Provide transportation and recreation opportunities near schools.
- C. Promote schools as focal points for neighborhood areas and as planning elements for new growth areas.

**POLICIES AND STANDARDS**

- 1.51 Coordinate school location and site design with the School District according to the following guidelines to ensure that adequate facilities are available.

**Elementary Schools**

Description: Facilities for 500 to 750 students in grades K through 6.

Location: Interior residential areas at a collector/local intersection. Additional street frontage is desired for transition area to adjacent residences. Abuts neighborhood park with adjacent development backing or siding onto school. Maximize pedestrian and bicycle access and on/off circulation.

Service Area: 1/2-mile radius to serve a population of 5,000 to 8,000.

Site Area: 15 to 20 acres.

Facilities: Approximately 20 classrooms, administration building, library, multi-purpose building (lunches, recreation, and community meetings), multi-purpose recreation/open space with hard-court play areas and equipment, off-street parking, bus loading/unloading area, and bicycle storage

A Specific Plan, under Article 8 of the Government Code, is a legal tool for detailed design and implementation of a defined portion of the area covered by a General Plan. A Specific Plan may include all detailed regulations, conditions, programs, and/or proposed legislation which may be necessary or convenient for the systematic implementation of any General Plan element(s).

area. Security fencing separates buildings from play areas. Public use of play areas is encouraged.

programmed streets, sewerage, storm drainage systems and other necessary infrastructure.

### **Middle Schools**

Description: Facilities for 700 to 1,000 students in grades 7 and 8.

Location: Residential areas with central location for surrounding elementary schools at collector/collector or collector/local intersections. Additional local street frontage desired for transition to adjacent residential areas. Maximize pedestrian and bicycle access and on/off-site circulation.

Service Area: 25 to 30 acres.

Facilities: Approximately 30 classrooms and labs; administrative center, library/media center; multi-use buildings, (cafeteria, band, chorus, shops, labs); athletic facilities for football, baseball, track; off-street parking; bus loading/unloading area; and bicycle storage area. Security fencing separates buildings from athletic fields. Public use of athletic fields is encouraged.

### **High Schools**

Description: Facilities for 1,000 to 1,400 students in grades 9 through 12.

Location: Arterial-collector intersection with additional frontage on two other streets. Prefer same collector as area middle school.

Service Area: 50 to 60 acres.

Facilities: Approximately 40 classrooms and labs; library/media center; administration building; gym; cafeteria; standard outdoor athletic facilities; off-street parking, bus loading/unloading; bicycle storage area. Security fencing separates buildings from athletic fields/facilities. Public use of athletic areas is encouraged.

- 1.52 Discourage and restrict commercial development that conflicts with school facilities.
- 1.53 Restrict development of High Density Residential complexes abutting school sites.
- 1.54 The School District shall coordinate its school location, facility construction and phasing with the City's development guidelines contained in the General Plan and the City's Capital Improvement Program to ensure that school facilities are located in areas where there are planned and



## 2.0 CIRCULATION ELEMENT

### INTRODUCTION

In the City of Dinuba, regional vehicular transportation is provided primarily by El Monte Way and Alta Avenue. El Monte Way runs in an east-west direction and Alta Avenue runs in a north-south direction. Regional rail is available through the Union Pacific railroad. Dinuba is served by a City transit system and Dial-a-Ride system.

### PURPOSE OF THE CIRCULATION ELEMENT

The Circulation Element guides the continued development and improvement of the circulation system to support existing and planned development, while the Land Use Element identifies the City's planned development pattern. The development of additional land in the future will increase the demand for local and regional roadway improvements and construction. The Circulation element establishes acceptable roadway service levels and identifies improvements required to maintain the service levels. The use of other modes of transportation such as transit, walking, and bicycling is promoted to reduce the demand for transportation system improvements and to improve air quality. The pedestrian and bicycling systems will also be used to connect the various activities centers identified in the Land Use Element and promote a pedestrian/bicycle friendly community.

The purpose of the Circulation Element is to provide a safe, efficient, and adequate circulation system for the City, State planning law requires: "...a circulation element consisting of the general location for proposed major thoroughfares, transportation routes, terminals, and other local public utilities and facilities, all correlated with the land use element plan." To meet this purpose, the Circulation Element addresses the circulation improvements needed to provide adequate capacity for future land uses. The Element establishes a hierarchy of transportation routes with typical development standards described for each roadway category. Reference the map pocket for the General Plan Circulation Map.

### SCOPE AND CONTENT OF THE CIRCULATION ELEMENT

The state General Plan Guidelines recommend that the circulation policies and plans should:

- Coordinate the transportation and circulation system with planned land uses;
- Promote the safe and efficient transport of goods and the safe and effective movement of all segments of the population;

- Make efficient use of existing transportation facilities; and
- Protect environmental quality and promote the wise and equitable use of economic and natural resources.

The Guidelines indicate that the Circulation Element should address all facets of circulation including streets and highways, transportation corridors, public transit, railroads, bicycle and pedestrian facilities, and commercial, general, and military airports. The Dinuba Circulation Element fulfills state requirements with a plan to provide effective circulation facilities supporting desired community development. Along with circulation, public utilities must be addressed in the General Plan. Instead of addressing utilities within the Circulation Element, the Dinuba General Plan contains a Public Services and Facilities Element that discusses the provision of utilities and public services/facilities.

This element contains goals, objectives, and policies and standards to improve overall circulation in Dinuba. For vehicular transportation, a hierarchical roadway network is established with designated roadway types and design standards. The roadway type is linked to anticipated traffic levels, and acceptable levels of service are established to determine when capacity improvements are necessary. Because local circulation is linked with the regional system, the element also focuses on participation in regional programs to alleviate traffic congestion and construct capacity improvements. Alternative transportation modes are also emphasized in this element to reduce dependency on the automobile and thereby improve environmental quality.

### GOAL

It is the overall goal of the City to design and maintain a fully integrated local network that provides for safe and convenient circulation using a variety of transportation modes.

## 2.1 ROADWAY CLASSIFICATION, STANDARDS

### OBJECTIVES

- A. Develop a circulation network of local roads, collectors, arterials that will meet projected traffic needs.



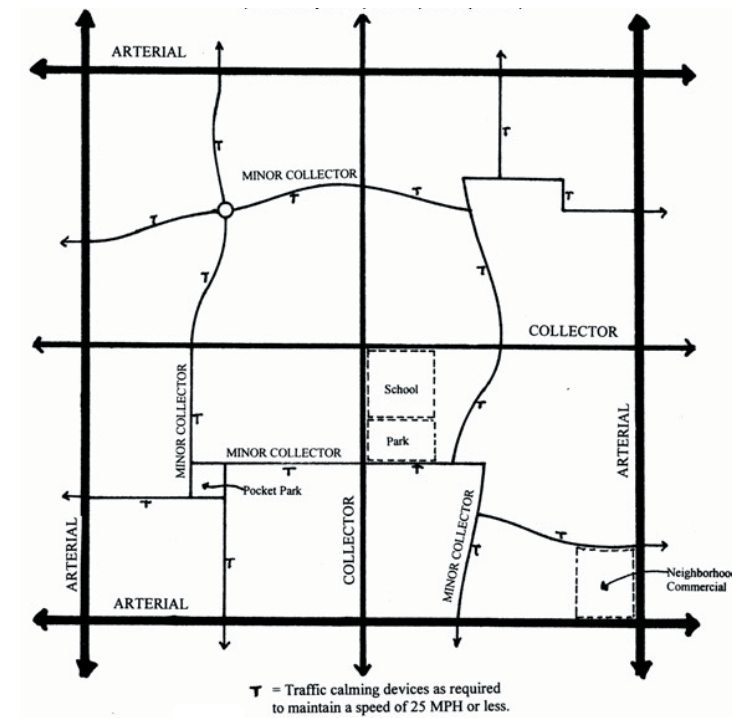
- B. Maintain a roadway level of service (LOS) of C or better on Local, Minor Collector, Collector and Arterial streets.

**POLICIES AND STANDARDS**

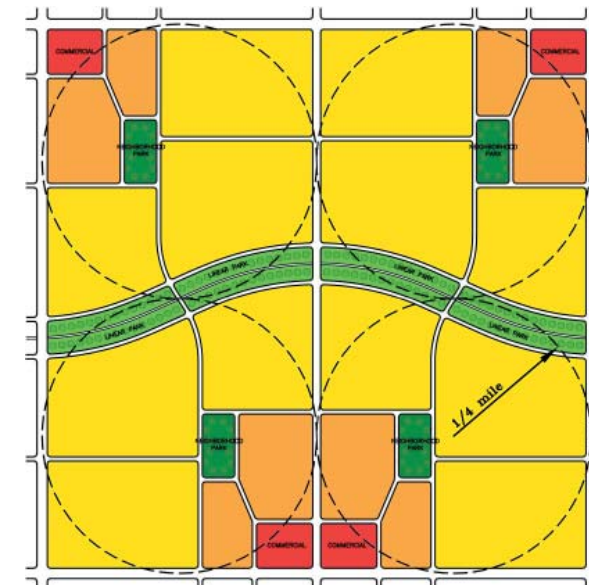
- 2.1 All street and roadway improvements shall be in conformance with the Circulation Plan contained in the General Plan Map (reference map pocket).
- 2.2 The Circulation Plan shall act as a guide in determining the function of major streets. The City's functional street classification system shall include major arterials, arterials, collectors, minor collectors and local streets.
- 2.3 Designate streets according to the following functional classifications:
  - a. Arterials serve as the principal network for cross-town traffic flow. They connect areas of major traffic generation within the urban areas and connect with important county roads and state highways. They also provide for the distribution and collection of through-traffic to and from collector and local streets serving residential, commercial, and industrial areas.
  - b. Collectors provide for traffic movement between arterial and local streets, traffic movement within and between neighborhoods and major activity centers, and limited direct access to abutting properties.
  - c. Minor collectors provide for pedestrian and vehicle movements between neighborhoods.
  - d. Local streets provide for direct access to abutting properties and for very localized traffic movements within residential, commercial and industrial areas.
- 2.4 Apply consistent standards for new street development, based on traffic carrying capacity and classification.
- 2.5 The design of arterials, collectors, minor collectors, and local streets shall comply with the Standard Drawings and Specifications Manual of the City of Dinuba, as amended.
- 2.6 The right-of-way widths and construction widths of all classes of streets from local to arterial shall be updated as necessary to reflect the street classifications in the Circulation Element.
- 2.7 The overall circulation plan for future neighborhoods shall be in conformance with Figures 2-1 and 2-2 and include off-set minor collectors, traffic calming features as needed, a neighborhood park within 1/4 mile walking distance per neighborhood, and a commercial/office/transit node.

*Level of Service (LOS) is a scale that measures the amount of traffic that a roadway or intersection can accommodate, based on such factors as maneuverability, driver dissatisfaction, and delay.*

**Figure 2-1**  
Overall Conceptual Circulation Plan  
(Illustrative only, refer to policies for precise requirements)



**Figure 2-2**  
Traditional Neighborhood Development Model



**ARTERIALS**

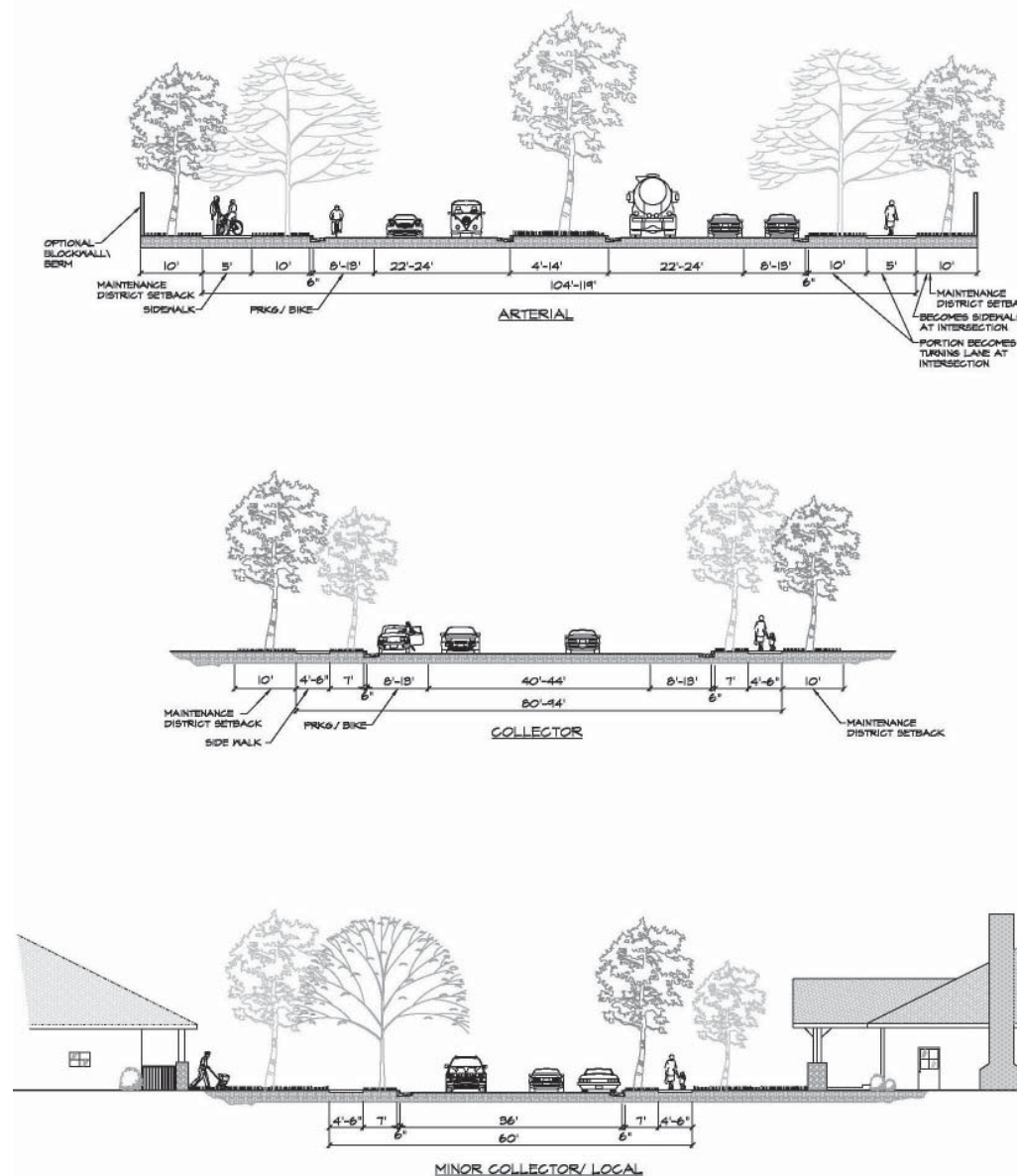
- 2.8 Alta and El Monte shall be developed with a minimum right-of-way of between 104'-119', to include four travel lanes, parking and/or bicycle lanes, and a two-way left center turn lane or landscaped median. Where widening these two arterials is neither feasible or practical, the right or way shall



remain 96 feet. Other arterials (Kamm, Crawford, Nebraska, Road 72, Road 64 and Road 96) shall be developed with a minimum right-of-way of 104 feet, to include four travel lanes, parking, and a two-way left center turn lane or landscaped median. Reference Figure 2-3.

- 2.9 The primary purpose of arterials is to carry traffic. Parking should be prohibited on new arterials and discouraged along existing arterials as deemed appropriate by the City Council and as traffic safety conditions warrant.
- 2.10 Arterials shall be built in areas where traffic demand warrants the development of this facility to meet the adopted level of service standard.
- 2.11 Arterial streets shall be built at a typical separation of one (1) mile. Major arterials shall be provided at two mile intervals.

**Figure 2-3  
Street Standards**



**COLLECTORS**

- 2.12 Collectors are to be designed with an 80-94 foot right-of-way width which allows four individual travel lanes of traffic, or two lanes with a two-way left turn center lane. Minor collectors may be developed with a 58-foot right-of-way to include two travel lanes, a two-way left turn center lane, and parking. (Reference Figure 2-3).
- 2.13 Collector streets shall be at approximately one-mile intervals centered between arterial streets and shall be planned to intersect with other streets so as to maximize traffic safety and discourage fast flowing-traffic through residential areas. Where possible, major arterials, minor arterials, and collectors shall form 4 leg, right angle intersections; jog, offset and skewed intersections of streets in near proximity shall be avoided where possible.
- 2.14 Minor collectors shall serve residential neighborhoods and provide for circulation between adjacent neighborhoods, but shall not be used to carry through traffic or high traffic volumes. Actual design and improvement to ultimate standards shall be achieved through development of small facilities by developers as areas adjoining the designated circulation system are developed, with allowance for bicycle lanes, where planned.

**OTHER STREET STANDARDS**

- 2.15 Local residential street right-of-way shall be a minimum of 60 (36' minimum curb to curb) feet which allows two travel lanes, parking, a parkway strip (7' wide), and sidewalk (4.5' wide).
- 2.16 Arterial, collector and local street standards shall be developed which provide adequate capacity for their appropriate function, and these shall be incorporated into the City's Standard Drawings and Specifications Manual. Half streets shall not be permitted.
- 2.17 Local streets shall not carry an unreasonable level of through traffic. If it is determined that a local street is carrying an unacceptable level of through traffic, the City may use appropriate means to reduce traffic through creation of one way traffic flow, installation of traffic calming devices, and/or any other means deemed to be acceptable under the Vehicle Code of the State of California. Traffic calming features in conformance with Table 2-1 are encouraged.
- 2.18 The General Plan Circulation Plan shows the street system consisting of arterials and collectors. Designated arterials and collectors are as follows:



**Table 2-1  
Permitted Traffic Calming Measures**

	Functional Classifications					Subdivision Streets			
	Interstates Freeways Expressways	Arterials	Collectors	Minor Collectors	Local Roads	Collector Streets	Minor Collector Streets	Local Streets	Other Restrictions
<b>Volume Control Measures</b>									
Full Closure Half Closure	Not Recommended				Only on an exception basis	Not Recommended		>500 vpd >25% non- local traffic	
Diagonal Diverter Median Barriers Forced Turn Islands	Not Recommended				<5,000 vpd >25% non- local traffic	Not Recommended		>500 vpd >25% non- local traffic	
<b>Vertical Speed Control Measures</b>									
Speed Humps	Not Recommended				Only on an exception basis	Daily volumes <3,000 vpd Posted speed <30 mph			Not on primary emergency routes or bus routes
Speed Tables Raised Crosswalks Raised Intersections	Not Recommended					Daily volumes <5,000 vpd Posted speed <35 mph			Not on primary emergency routes
<b>Horizontal Speed Control Measures</b>									
Mini-traffic Circles	Not Recommended				Only on an exception basis	Daily volumes <5,000 vpd Posted speed <35 mph			Not on primary emergency routes or bus routes
Roundabouts	Not Recommended					Combined approaches daily volumes <5,000 vpd Posted speed <35 mph			
Lateral Shifts	Not Recommended					Daily volumes <10,000 vpd Posted speed <35 mph			
Chicanes	Not Recommended					Daily volumes <5,000 vpd Posted speed <35 mph			
Realigned Intersections	Not Recommended					Daily volumes <5,000 vpd Posted speed <35 mph			
<b>Narrowings</b>									
Bulbouts Two-Lane Chokers Center Islands	Not Recommended					Daily volumes <1,000 vpd Posted speed <35 mph			
Combined Measures	Not Recommended					Subject to limitations of component measures			

Note: Subject to Approval by City Engineer

**Arterials**

- Alta Avenue
- Crawford Avenue
- El Monte Way
- Kamm Avenue
- Nebraska Avenue
- Road 72
- Road 96
- Road 64
- Avenue 400
- Road 56
- Road 68 (between El Monte and Kamm)

**Collectors**

- Alice Avenue/Monte Vista Drive
- College Avenue/Road 84
- Euclid Avenue
- "K" Street
- Lincoln Avenue
- "M" Street
- Uruapan Drive
- Road 92
- Saginaw Avenue
- Sierra Way
- Tulare Street
- Road 68
- Avenue 404
- Road 76
- Avenue 428

2.19 Median breaks and driveway standards for arterial, collector and local streets directly affect the performance of these roadways, and the following minimum standards have been developed to facilitate the proper operation of these roadways:

**ARTERIAL STREET STANDARDS**

- a. Driveway access to major activity centers should be located no closer than 200 feet to the adjacent intersection of a collector or arterial street. (Measurement shall be from the curb return to the nearest edge of the driveway). If driveways must be provided near intersections for facilities (such as service stations) these driveways shall not be serviced by median breaks and shall be located no less than 50 feet from the intersection (measurement shall be from the curb return to the nearest edge of the driveway). If more than one driveway is required to serve a property, the driveways shall be separated by 50 feet (The 50 feet is to be measured edge to edge, not centerline to centerline).
- b. The distance between driveways along commercially developed arterials should not be less than 400 feet (measurement shall be from centerline to centerline). Where this spacing is not practical, the development shall provide acceptable traffic mitigation measures in addition to those already required.
- c. Where practical and desirable, driveways should be located on adjacent collector streets rather than on arterial streets.
- d. Driveway consolidation shall be encouraged through joint access agreements along arterials where standards a. through e. are exceeded.
- e. Full median breaks, where there is no adopted design, should provide access to collector streets and to major activity centers and should not be less than 200 feet from an adjacent intersection of an arterial or collector street, and not less than 1,000 feet between full median breaks on major arterials.
- f. Arterial streets shall include landscaping in accordance with Figure 2-3.

**COLLECTOR STREET STANDARDS**

- a. Driveway access to major activity centers should be located no closer than 150 feet to the adjacent intersection of a collector or arterial street (measurement shall be from the curb return to the nearest edge of the driveway). If driveways must be provided near intersections for facilities (such as service stations) these driveways shall not be serviced by median breaks and shall be located no less than 50 feet from the intersection (measurement shall be from the curb return to the edge of the driveway). If more than one is requested to serve a property, the driveways shall be separated by 50 feet, measured edge to edge, not centerline to centerline.

- b. The distance between driveways and intersecting local streets should not be less than 300 feet. (Measurement shall be from the curb return to the nearest edge of the driveway). Where this spacing is not practical, the development shall provide acceptable traffic mitigation measures in addition to those already required.
  - c. Driveways to residential property along collectors should be consolidated whenever possible.
  - d. Concrete medians shall be provided on collectors where left turn control is needed and by painted medians on two way left turn pockets where appropriate. Where concrete medians are provided, median breaks should be spaced not less than 300 feet apart.
  - e. Collectors shall include landscaping in conformance with Figure 2-3.
- 2.20 The street network should provide a quick and efficient route for emergency vehicles, including police, fire and other vehicles, when responding to calls for service. The length of single entry access routes shall be restricted.
- 2.21 Standards for new street development can be altered or refined through the specific plan or planned unit development process where it can be demonstrated that projected traffic flows can be accommodated.
- 2.22 New street development in areas of urban expansion should not be limited to a linear “grid system”. Varied street layouts may be permitted where they are in conformance with the Circulation Plan. Private streets and/or gated communities may be considered under the Planned Development process.
- 2.23 City policy is to continue to provide a high level of service to the community. Therefore, the City designates Service Level “C” (ADT) as defined in the Highway Capacity Manual (published by the Transportation Research Board of the National Research Council) as the minimum desirable service level at which arterial and collector streets should operate. All new facilities in these categories shall be designed to operate at this level or better for a period of at least 20 years following their construction. Level of Service “D” (peak hours) shall be deemed acceptable for those road segments and intersections which have been identified as already operating at that level.
- 2.24 Traffic studies should be required, when necessary, to determine mitigation measures necessary to mitigate traffic impacts.
- 2.25 Intersection improvements should be made to the existing major street system selectively through traffic engineering solutions rather than major structural improvements. This could include signalization, intersection channelization, use of directional signs, and diversion of traffic onto underutilized streets, or through sequential traffic signal timing.
- 2.26 City circulation system street alignments shall be coordinated with Tulare County circulation system street alignments.
- 2.27 In order to promote safe and efficient traffic flow throughout the City, traffic signals shall be spaced no closer than 1/4 mile on arterials except in unusual circumstances. The intersections of arterial and collector streets and the access driveways to major traffic generators shall be located so as to maintain this minimum spacing.
- 2.28 Tulare County should incorporate Dinuba’s Circulation Element into its County-wide General Plan.
- 2.29 Promote the improvement of El Monte Way (Avenue 416) as the primary transportation access to the City from Highway 99. To this end, the City will cooperate with Tulare County, Fresno County, and CalTrans to prepare plan lines and secure funding for right-of-way, additional lanes, signalization, and streetscape improvements.
- 2.30 The ultimate development of El Monte Way (Avenue 416) should include four travel lanes from Highway 99 through the City to Road 100.
- 2.31 Encourage CalTrans to improve directional signage to Dinuba from major Highway 99 exits.
- 2.32 The circulation system shall be designed and developed to minimize excessive noise impacts on sensitive land uses and traffic congestion which would increase the rate of vehicle emissions. Development shall mitigate noise and emission impacts.
- 2.33 Right-of-Way essential to the circulation system should be dedicated and/or developed to the appropriate extent and width when a zone change to a greater density, division of property or development occurs. The City shall have the County of Tulare apply the same requirements within the urban development boundary.
- 2.34 All land development proposals shall be reviewed to assure consistency with this Circulation Element.
- 2.35 Due to limited additional traffic carrying capacity of El Monte to east and west bound traffic, development should be encouraged which utilizes Nebraska and Kamm Avenues.

## 2.2 STREET IMPROVEMENTS

### OBJECTIVE

Protect rights-of-way for future street development by clearly defining the location of future rights-of-way and establishing street dedication requirements. Such methods should minimize adverse impacts on adjacent properties and avoid imposition of street improvement requirements significantly in advance of need.

### POLICIES AND STANDARDS

- 2.36 Adopt the official plan line process, as provided for in State Law, as the City's method of protecting rights-of-way for future street improvements.
- 2.37 Establish official plan lines for all arterial and collector streets included in the Circulation Element of the General Plan.
- 2.38 Official plan lines for El Monte Way, between Alta and Tulare, and for Alta Avenue, between El Monte and Vassar, should include potential abandonment or realignment of City streets which now intersect those streets at approximately a 45° angle.
- 2.39 Delay or transfer street improvement requirements resulting from the granting of an entitlement for properties located on future arterial or collector streets where the official plan line indicates delay of ultimate street improvements is appropriate.
- 2.40 Streets abutting new development shall be developed to handle bi-directional traffic and at least one parking lane. Arterial half streets may be permitted when additional lanes are not necessary to support development. Half collector and local streets shall not be permitted.

## 2.3 MAINTENANCE/CONSTRUCTION

### OBJECTIVE

Efficiently manage the construction and maintenance of the street and highway system.

### POLICIES AND STANDARDS

- 2.41 Develop a 5-year Capital Improvement Plan to identify and provide adequate sources of funding for both maintenance and improvement of the street and highway system.

- 2.42 Develop a traffic monitoring system to assist in establishing a priority system for expending street and highway funds.

## 2.4 TRAFFIC SAFETY

### OBJECTIVE

Maximize the use of site planning techniques to improve traffic safety.

### POLICIES AND STANDARDS

- 2.43 Limit vehicular access from residential developments along arterials by requiring development to back-on to such streets (with ornamental fencing, landscaping and waiver of access).
- 2.44 Allow direct access to collector streets with 2,000-4,000 average daily traffic counts from residential areas except where physical circumstances do not allow other design solutions, or where opportunities exist to consolidate points of access for businesses and/or residences.
- 2.45 Require the use of street-type driveway approaches on collector and arterial streets for any development containing 20 or more parking spaces.
- 2.46 Promote and develop design standards for local streets to reduce right-of-way width and paving, and to utilize parkway strips.
- 2.47 Provide left hand-turn lanes where necessary for access from arterials into high traffic commercial or multifamily developments.
- 2.48 Project designs shall reflect options for reducing through traffic on local streets.
- 2.49 Promote design standards which allow for safe and efficient transport, delivery, loading and unloading of goods from service vehicles within commercial and industrial areas.
- 2.50 Develop street patterns for interior streets within new subdivisions to integrate neighborhoods and provide for continuous access routes. The City should discourage residential developments which have self contained street circulation standards.
- 2.51 Where major new activity centers are proposed along arterial and collector streets, designs shall be encouraged which minimize construction along the property line or along the adopted set back line, whichever is appropriate.



- 2.52 Developers shall mitigate traffic impacts associated with their projects to minimize the impacts to major arterials, arterials, and collector streets.
- 2.53 The City shall promote an active policy of consolidating driveways, access points and curb cuts along existing major arterials, or arterials when development or change in intensity of development or land use occurs or when traffic operation or safety warrants.
- 2.54 Residential subdivisions shall be designed to encourage access from collector streets or minor collectors and discourage the use of local streets to bypass congested arterials.
- 2.55 Where arterial and collector streets are required, residential development shall be oriented away (side on or rear on) from such streets, and properly buffered so that the traffic carrying capacity on the street will be preserved and the residential environment protected from the adverse characteristics of the street.
- 2.56 Due to the traffic congestion which results from numerous points of ingress and egress along commercial streets, future commercial developments or modifications to existing developments shall be designed with limited points of ingress and egress onto a major street. Ingress and egress to shopping centers should be carefully designed in order to promote traffic safety. Left hand movements into and out of commercial areas should be minimized and existing points of ingress and egress shall be consolidated whenever possible.

**2.5 ALTERNATIVE TRANSPORTATION MODES**

**OBJECTIVE**

Promote the use of alternative modes of transportation.

**TRANSIT**

**POLICIES AND STANDARDS**

- 2.57 Coordinate transit services with surrounding cities, the County of Tulare, Tulare County Association of Governments (TCAG), and the Transportation Planning Agency.
- 2.58 Cooperate with the TCAG in providing transit service and planning to meet the social and economic needs of all segments of the community.
- 2.59 Provide reasonable accommodations for comfort and convenience for riders at major transit destinations so people can utilize the transit sys-

tem safely and comfortably. The City shall determine such need based on site plan review procedure and other planning implementation methods.

- 2.60 Major arterials, arterials, and collectors will be designed to allow transit vehicles to pull out of traffic. This policy may be implemented with either a continuous parking lane with bus stops, or with special bus pull out lanes.
- 2.61 Transit centers/stops shall be established to encourage the interface between commercial centers, alternate transportation modes, high density residential uses and the transit system.
- 2.62 Encourage transit alternatives to meet the basic transportation needs of the young, the elderly, the handicapped, and individuals without access to an automobile.
- 2.63 Maintain opportunities for a transit center within the City where alternative transit modes would connect.
- 2.64 Encourage and provide for ride sharing, park and ride, and other similar commuter energy savings programs.

**2.6 BICYCLE FACILITIES**

**OBJECTIVE**

Encourage the use of bicycles as a viable means of transportation.

**POLICIES AND STANDARDS**

- 2.65 The City will develop, through various funding mechanisms and sources, a city-wide bicycle/pedestrian path system. The bicycle/pedestrian path system will utilize existing or future railroad right-of-way and water courses such as Traver Creek. The path, which would be between 8 and 12 feet wide and off the roadway, could also include landscaping, lighting, mileage markers and directional signage and benches. Reference Figure 2-4 for the proposed bicycle/pedestrian city-wide path system.
- 2.66 The City of Dinuba will work with the City of Reedley to connect the city-wide bicycle/pedestrian trail system with a similar path system in Reedley.
- 2.67 Provide bikeway signage for Regional Bike Routes.

2.68 Support the installation of bicycle parking racks at public and private places of assembly such as parks, schools, office buildings, churches, and retail commercial developments.

2.69 Promote bicycle safety education programs in elementary schools through the police and recreation departments.

**2.7 PEDESTRIAN FACILITIES**

**OBJECTIVE**

Provide a safe walking environment for pedestrians.

**POLICIES AND STANDARDS**

2.70 Sidewalks, paths, and appropriate crosswalks should be located to facilitate access to all schools and other areas with significant pedestrian traffic. Whenever feasible, pedestrian paths should be developed to allow for unobstructed pedestrian flow from within a neighborhood.

2.71 Sidewalks shall be required in all areas of the community to accommodate pedestrian traffic, especially along routes with high pedestrian traffic such as schools, parks, and the Downtown area. Installation of these improvements shall be encouraged to the extent feasible in existing neighborhoods where they do not currently exist.

2.72 Where security walls or fences are proposed for residential developments along major arterials, arterials, or collector streets, pedestrian access will be provided between the major arterial, arterial, or collector, and the development to allow access to transit vehicles, commercial facilities, educational facilities, and recreation areas. Pedestrian access to arterial streets is encouraged. Such access should be located every 300 to 400 feet.

2.73 Street lighting shall be provided for all public streets.

2.74 Pedestrian signals should be provided at all traffic signal locations.

2.75 Adequate sidewalk and park strip maintenance should be assured. Owners of abutting property shall be responsible for maintenance of sidewalks and park strips. City shall work to assist property owners, where possible.

**2.8 RAIL SERVICE**

**OBJECTIVE**

Assure the continuation of railroad freight service to the City Dinuba.

**POLICIES AND STANDARDS**

2.76 To preserve the viability of the Union Pacific rail corridor, uses or activities shall not be permitted to encroach so as to reduce the efficiency of the rail system.

**2.9 SAFETY STANDARDS**

**OBJECTIVE**

Upgrade and maintain existing transportation corridors to meet urban safety standards.

**POLICIES AND STANDARDS**

2.77 Encourage the development of improved signalization and intersection design.

2.78 Utilize traffic control devices such as center medians and/or left turn pockets where appropriate and feasible.

2.79 Adequate street lighting and traffic control devices should be provided throughout the City to ensure safe and efficient mobility.

2.80 Signals should be timed and coordinated, where possible, to increase intersection performance, reduce truck traffic impacts and provide for efficient cross town traffic circulation.

**2.10 PARKING**

**OBJECTIVE**

Promote a parking program that meets the needs of each land use type.

**POLICIES AND STANDARDS**

2.81 Adequate off-street parking shall be required of all commercial and industrial



land uses to accommodate parking demand. Off-street parking shall also be required (and be used) of residential land uses to accommodate tenants.

- 2.82 Parking standards shall be evaluated for new downtown developments to ensure that parking requirements are satisfied within walking distance of such development.
- 2.83 Parking standards shall be evaluated to assess the potential for offering reduced parking requirements to developments that incorporate measures proven to reduce vehicular trips. Shared parking should be encouraged wherever possible.

## 2.11 TRANSPORTATION SYSTEM AND CONGESTION MANAGEMENT

### OBJECTIVE

Encourage the development of strategies for maximizing the efficiency of the existing street system.

Promote a variety of public transit connections with other nearby cities and locations.

### POLICIES AND STANDARDS

- 2.84 The City shall encourage the use of energy efficient and non polluting fuels and modes of transportation.
- 2.85 Transportation System Management and Transportation Demand Management strategy should be used to mitigate traffic and parking congestion. Public transit, traffic management, ridesharing and parking management are to be used to the greatest extent practicable to implement transportation management strategies.
- 2.86 Promote the long term shifting of peak hour commute trips from the single occupant automobile to ridesharing, buses, pedestrian, and bicycles.
- 2.87 Large developments shall be encouraged to incorporate transit passenger facilities, bicycle racks, lockers, shower facilities, as well as on site services (eating, mail, banking, etc.) as ways to reduce vehicle trips.
- 2.88 Investigate modifying the contract with Dinuba Transit to initiate regular bus service to Reedley and Selma.
- 2.89 Plan for obtaining the right to construct a bicycle/pedestrian path along the Southern Pacific Railway line, should it be abandoned.

## 2.12 MAINTENANCE AND INTEGRATION

### OBJECTIVE

Develop adequate maintenance programs for the community's transportation networks.

### POLICIES AND STANDARDS

- 2.90 The community's transportation infrastructure, including streets, street lights, sewer, water, storm drains, and facilities shall be adequately maintained. The City shall maintain their facilities and encourage other utilities to adequately maintain their facilities.
- 2.91 The maintenance of the investment in the existing and future infrastructure is a high priority for the community.
- 2.92 The City shall maintain a high level of inter governmental coordination and citizen participation in the circulation and transportation planning process and work with other agencies to assure that regional transportation plans are consistent with the City's General Plan.

## 2.13 TRUCK ROUTES AND TRUCK PARKING

### OBJECTIVE

Provide safe and efficient truck routes into and within the community.

### POLICIES AND STANDARDS

- 2.93 Truck traffic shall be permitted on the designated arterials and collector streets only; as identified in the Circulation Element Truck Route Map (reference Figure 2-5).
- 2.94 Truck parking
- Shall be discouraged on arterial/collector streets outside of the industrial park.
  - Shall be prohibited in residential areas for vehicles in excess of 10,000 gross vehicle weight (GVW), or higher than 8 feet.
- 2.95 The City shall encourage development of a truck terminal and parking facilities within the industrial park.



Figure 2-4

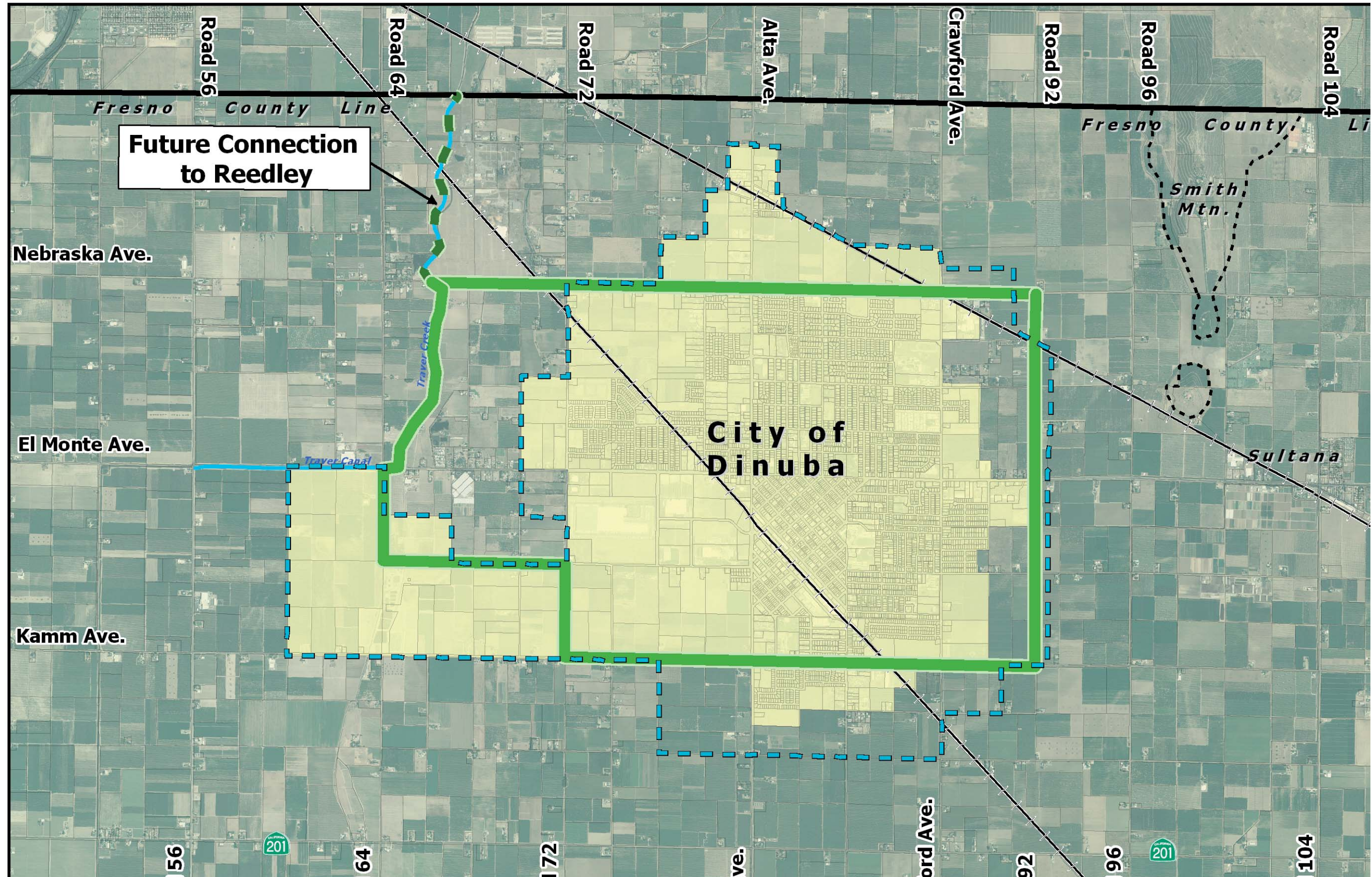
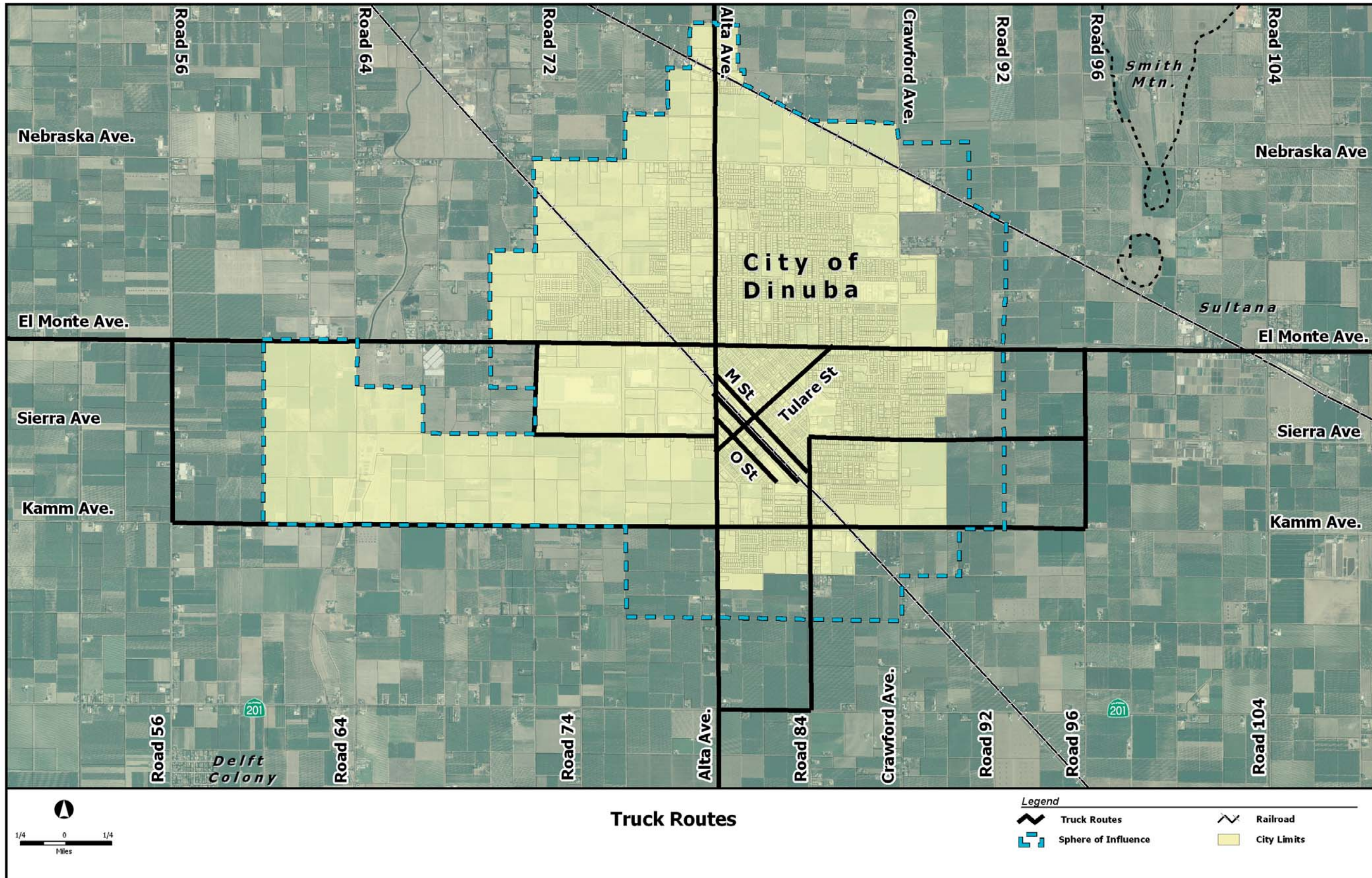




Figure 2-5





### 3.0 OPEN SPACE, CONSERVATION AND RECREATION ELEMENT

#### INTRODUCTION

Some of the most valuable assets of Dinuba include its agricultural land, parks, historical and architectural resources. The Open Space, Conservation and Recreation Element focuses on the protection and enhancement of open space, natural and recreational resources to ensure a high quality living environment in Dinuba.

#### PURPOSE OF THE OPEN SPACE, CONSERVATION AND RECREATION ELEMENT

The Open Space, Conservation and Recreation Element meets the state requirements for Conservation and Open Space Elements as defined in Sections 65302(d) and 65301(e) of the Government Code. According to these requirements, the Conservation Element must contain goals and policies to protect and maintain natural resources such as water, soils, wildlife, and minerals, and prevent wasteful resource exploitation, degradation, and destruction. The Open Space Element must contain goals and policies to manage open space areas, including undeveloped lands and outdoor recreation areas. Specifically, the Open Space Element must address several open space categories such as those used for the preservation of natural resources and managed production of resources, as well as open space maintained for public health and safety reasons. This last category of open space is addressed in the Safety Element. Because the subjects required to be addressed under the Conservation Element and Open Space Element overlap substantially, the two elements, and the Recreation Element have been combined for this Plan.

#### SCOPE AND CONTENT OF THE OPEN SPACE, CONSERVATION AND RECREATION ELEMENT

The Open Space, Conservation and Recreation Element includes community policies to protect environmental, open space and recreational resources. Resources addressed in this element include: water resources; agricultural resources; cultural resources; ecological and biological resources; mineral resources; and parks and recreational facilities. Because everyday activities in Dinuba affect air quality outside City boundaries and regional activities affect air quality within Dinuba, regional air quality issues are also addressed in this element.

The Open Space, Conservation and Recreation Element is comprised of four sections: the Introduction; Purpose of the Open Space, Conservation and Recreation Element; Scope and Content of the Open Space, Conservation and Recreation Element; and the Objectives, Policies and Standards. In the Objectives, Policies and Standards section, community open space needs and



resource management issues are identified and corresponding policies are established. The objectives, which are overall statements of the City desires, are comprised of broad statements of purpose and direction. The policies serve as guidelines for planning and maintaining recreational facilities, enhancing the natural amenities of Dinuba and minimizing the environmental effects of planned development.

#### 3.1 AGRICULTURE

##### OBJECTIVES

- A. To preserve prime farmland, farmland of statewide importance and farmland of local importance within the Dinuba Planning Area to support continued agricultural production.
- B. To provide a greenbelt around the City's perimeter to maintain the physical separation between the City of Dinuba and nearby communities, and to maintain the scenic beauty surrounding the City. The City should also establish hard edge growth phasing boundaries such as roadways, railroad right of ways, irrigation ditches, etc. to protect agriculture.

##### POLICIES AND STANDARDS

- 3.1 Assure the continuation of agricultural production as an important economic activity by establishing areas primarily north and east of the City to be designated and maintained as part of the City's greenbelt.
- 3.2 New residential development shall be substantially contiguous to existing development. Development should not occur unless at least 35% of a parcel is contiguous to existing urban development. This measure is intended to help reduce the unnecessary removal of finite natural re-

<b>Farmland Preservation Tools</b>		
<b>Tools</b>	<b>Duration</b>	
	<b>Short to Medium Term</b>	<b>Long Term</b>
Agricultural Districts		X
Differential Assessment	X	
Right-to-Farm Law		X
Agricultural Zoning		X
Urban Growth Boundary	X	
Donation of Development Rights		X
Purchase of Development Rights		X
Transfer of Development Rights		X



sources, such as prime soil, to reduce the cost of community services provided to residents, and to eliminate leap-frog development.

3.3 Extension of urban improvements and services, including water and sewer lines and storm drain facilities, into agricultural areas shall be managed as a means to direct the location and timing of new urban development.

3.4 The City will give preference to new development projects that are proposed for non-prime agricultural soils.

3.5 To protect human health from potential impacts due to agricultural spraying, dust, and traffic congestion, the City will encourage lower density developments adjacent to land planned for long-term agricultural uses.

3.6 Maintain a 20-acre minimum parcel size for agriculturally designated parcels to encourage viable agricultural operation and to prevent parcelization into rural residential or ranchette developments.

3.7 Increase residential densities through integration of small-scale or corner lot duplexes into areas designated for single family development, thereby reducing the need for conversion of prime agriculture land.

3.8 The City shall work with Fresno County, the City of Reedley and Tulare County to devise a Specific Plan for the greenbelt area along the northern edge of Dinuba. The intent of this Specific Plan is to accomplish the following:

a. To maintain an area of extremely low density development along the Fresno County line. This area should be located on both sides of the Fresno County line, where feasible. Density should not exceed one dwelling unit per 10 acres. Uses should be restricted to agriculture, and agricultural-service industries.

b. The Specific Plan should identify potential regional public recreation facilities, such as a park and bicycle/pedestrian trail system. This system should be connected to a similar system within the City.

c. In order to protect the landowners within the buffer area, a funding mechanism should be developed and implemented which would allow the City to purchase development rights, open space easements, or properties, so as to protect the area in perpetuity.

## 3.2 NATURAL RESOURCES

### OBJECTIVES

A. To protect natural resources including groundwater, soils, and air quality, to

meet the needs of present and future generations.

B. Ensure that environmental hazards including potential flooding and impacts from agricultural practices are adequately addressed in the development process within the City and the Dinuba Urban Development Boundary.



### POLICIES AND STANDARDS

3.9 To protect human health, the City groundwater resources will be monitored on a regular basis to test for bacteriological and toxic chemical components.

3.10 Protect areas of natural groundwater recharge from land uses and disposal methods which would degrade groundwater quality. Promote activities which combine stormwater control, and water recharges.

3.11 The City will expand programs that enhance groundwater recharge in order to maintain the groundwater supply, including the installation of detention/retention ponds in new growth areas.

3.12 No development shall be approved in the City unless the development is, or can be served by the City sewer system.

3.13 Water conservation methods shall be continued.

3.14 Promote biological diversity and the use of plant species compatible with the bio-region.

3.15 Removed trees shall be replaced with tree species specified on the City's Tree Master Plan.

## 3.3 RECREATION

### OBJECTIVE

To provide recreational opportunities including local parks for all populations for the existing community, and projected population in future growth areas.

#### Water Conservation Methods

Numerous methods have been developed to implement water conservation.

##### Indoor Methods

Residential indoor methods can include the following:

- Purchasing energy- and water-efficient appliances
- Retrofitting toilets
- Installing automated shutoffs and timers on equipment that uses water
- Conducting leak detection and repair
- Installing on-demand hot water devices
- Installing low-flow water faucets and showerheads
- Installing submetering of multifamily residents
- Setting water rates that encourage conservation

##### Commercial and industrial indoor conservation methods include the following:

- Making cooling or heating system improvements
- Replacing water-cooled equipment with air-cooled units
- Using recycled process water
- Conducting leak detection and repair
- Reclaiming rainwater for toilet use
- Installing waterless urinals (where permitted)
- Installing domestic metering where none exists

##### Outdoor Methods

- Enacting a landscape planting ordinance that restricts allowable plantings of trees, shrubs, and grass
- Using native plants that may require less water
- Allowing the application of only as much water as the landscape needs
- Installing a rainwater harvesting system to capture rainwater runoff from roofs
- Using a hose nozzle, and not using a hose as a broom

Source: APA, Planning and Urban Design Standards, 2006

**POLICIES AND STANDARDS**

3.16 Where feasible, irrigation district facilities, such as ditch easements, should be used for pedestrian/bicycle paths.

3.17 Provide adequate parks facilities distributed throughout the City to provide organized and informal recreation opportunities and open space for City residents. Table 3-1, to be used as a reference only, classifies various parks and greenways, provides a general description and includes size and service area criteria.

In accordance with Section 2.6 “Bicycle Facilities” in the Circulation Element, the City will develop a city-wide bicycle/pedestrian path system which will utilize existing or future railroad right-of-way and water courses such as Traver Creek. The path, which would be between 8 and 12 feet wide, could also include landscaping, lighting, mileage markers and directional signage and benches. Reference Figure 2-2 in the Circulation Element for the proposed bicycle/pedestrian City-wide path system.

3.18 Provide recreation programs that meet the needs of children, adults and senior citizens in the City.

3.19 Provide an appropriate ratio of passive and active uses in each park type. Ensure that all parks provide the potential for passive, restful relaxation. A major portion of some parks shall be for passive activity while a portion of other parks may be for active recreation.

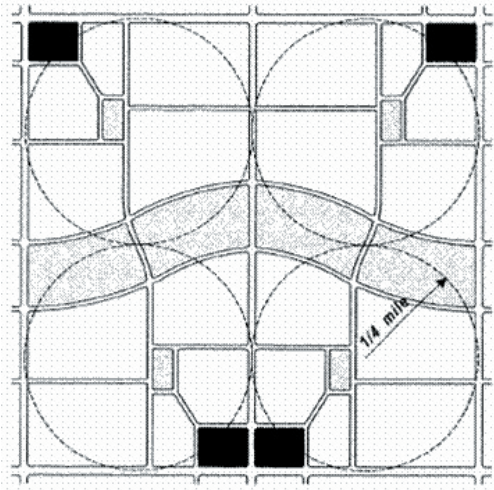
3.20 Improvements to existing parks shall be primarily in the form of upgrading the quality of existing facilities and improvements to accommodate new residents. This shall be achieved by either remodel or redevelopment. Facilities shall be constructed which are durable and require low maintenance, wherever possible.

3.21 Other improvements to existing parks shall be for the purpose of reducing maintenance cost, water use, improving safety and aesthetics.

3.22 In the Parks and Recreation Master Plan, there should be a balance between revitalization of existing facilities, parkland acquisition and development of new parks.

3.23 The standard park acreage for residents is 5.0 acres per 1000 people. This acreage may include School District property which is made available through cooperative agreements, park-ponds (to the extent that they are accessible and usable recreational areas), neighborhood parks, pocket parks, community parks and community recreational facilities. Priority should be given to development of property already owned by the City for park programs.

Figure 3-1



Future neighborhood parks should be centrally located within each 1/2 square mile of the City. These parks should be the focal point of neighborhoods.

Classification	General Description	Size and Service Area Criteria
Neighborhood Park	Neighborhood parks are the basic units of the park system and serve a recreational and social purpose. Focus is on informal recreation.	Typically 5 acres or more; 8 to 10 acres preferred with 3 acres the desired minimum size. Service area is one-fourth to one-half mile uninterrupted by major roads and other physical barriers.
Community Park	Serves a broader purpose than neighborhood parks. Focus is on meeting community-based recreational needs, as well as preserving unique landscapes and open spaces.	Varies, depending on function. A minimum of 20 acres is preferred, with 40 or more acres optimal. Service area can be communitywide or several neighborhoods in given area of the community.
Large Urban Park	Large urban parks are generally associated with larger urban centers with large populations. Focus is on meeting wide-ranging community needs and preserving unique and sometimes extensive landscapes and open spaces.	Varies depending on circumstances. A typical minimum size is 50 acres (20.2 hectares), with hundreds of acres not uncommon, such as Central Park in New York City.
Youth Athletic Complex/Facility	Consolidates programmed youth athletic fields and associated facilities to fewer strategically located sites throughout the community. Also can provide some neighborhood use functions.	Varies, with 20 acres or more desirable, but not absolute. Optimal size is 40 to 80 acres (16.3 to 32.4 hectares).
Community Athletic Complex/Facility	Consolidates programmed adult and youth athletic fields and associated facilities to a limited number of sites. Tournament-level facilities are appropriate.	Varies, with 20 acres (8.1 hectares) or more desirable, but not absolute. Optimal size is 40 to 80 acres (16.2 to 32.4 hectares).
Greenway	Lands set aside for preserving natural resources, remnant landscapes, and open space, and providing visual aesthetics/buffering. Also provides passive-use opportunities. Ecological resource stewardship and wildlife protection are high priorities. Suitable for ecologically sensitive trail corridors.	Varies, depending on opportunity and general character of natural systems within the community.
Parkway	Linear park like transportation corridors between public parks, monuments, institutions, and sometimes business centers. Can be maintained green space or natural in character.	Varies.
Special Use	Covers a broad range of parks and recreation facilities oriented toward single-purpose uses, such as a nature center, historic sites, plazas, urban squares, aquatic centers, campgrounds, and golf courses.	Varies, depending on need.
Park-School	School sites that are used in concert with, or in lieu of, other types of parks to meet community park and recreation needs. School sites often provide the majority of indoor recreational facilities within a community.	Varies, depending on specific site opportunities.
Private Park/Recreation Facility	Parks and recreation facilities that are privately owned, yet contribute to the public park and recreation system.	Varies.
Regional Parks and Park Reserves	Larger-scale, regionally bases parks and open spaces that focus on natural resource preservation and stewardship.	Typically a minimum of 500 acres (202.3 hectares) and up to several thousand acres or several hundred hectares. Service area is regional, which generally encompasses several cities.

Source: APA, Planning and Urban Design Standards, 2006



- 3.24 The City shall encourage future neighborhood parks (3-5 acres in size) to be centrally located within each section of land (reference Figure 3-1).
- 3.25 Where possible, parks should be developed in conjunction with school property to create a larger combined open space and recreation facility for the community and to reduce the costs for parks and recreation facilities.
- 3.26 Where possible, parks should be developed in conjunction with existing and future drainage basins to create a larger combined open space along with additional space for active and passive recreation. Existing park ponds should be adapted for park use where possible. Safety concerns must be addressed and adequate space at or above street level should be provided.
- 3.27 Park facilities should be provided in each quadrant of the City and should be within a ¼ mile walking distance of most residents.
- 3.28 Neighborhood park facilities may be contained within community parks.
- 3.29 Provide active recreation facilities in several locations in the City to accommodate community needs.
- 3.30 Community facilities of a specialized nature may be developed to service the particular interest of the community.
- 3.31 Not all community facilities should occur at each community park; they should be based on need, and should occur at various City parks.
- 3.32 The active community sports facilities should be lighted for extended hours of use when it does not conflict with adjacent land uses.
- 3.33 The majority of City parks should have some active recreational facilities. These facilities may be a single ballfield, a pair of tennis courts, a group of horseshoe pits or a group picnic area. At the maximum level, these facilities may include a complex of ballfields, a sports center, or a swimming pool.
- 3.34 Parks shall be protected from intrusion by other uses. Areas designated for park sites shall be preserved through zoning or the specific plan process. Alternative sites to those shown on the Land Use map may be permitted through a General Plan Amendment.
- 3.35 The City will review the Parks and Recreation Master Plan at least every five years to consider changing priorities and schedules for acquisition and development to implement the General Plan.

- 3.36 The City will coordinate with public schools, private industry and commercial developers to attain maximum use and minimum duplication in the cost of park and recreation facilities.
- 3.37 Where a county-wide recreation need is demonstrated in an area adjacent to the City, cooperative park development programs shall be encouraged on a cost-sharing basis. Joint power agreements between Dinuba and County agencies may be developed to implement such parks with financial aid management obligations in proportion to each agency's responsibilities.
- 3.38 If a subdivision, site plan, general amendment or rezoning is proposed on land which is designated for potential park use, prior to entitlements, permits or other approvals, the City Council shall determine the feasibility of accelerating public acquisition of the property, or redesignate alternative areas.
- 3.39 When a site designated for a park is part of a subdivision map, the City may require the subdivider to dedicate the park area and prepare plans for its phased development. Development of the park proposal shall be consistent with this element and the Parks and Recreation Master Plan.
- 3.40 Aggressively seek State, Federal, and local grants to improve City recreation services and facilities.
- 3.41 Maintenance costs should be within the City's financial ability. Where necessary, the City may require the developer to establish financing mechanisms.
- 3.42 Support the establishment of public non-profit corporations with the purpose of promoting and supporting City park and recreation services and facilities for the general public.
- 3.43 Continue to promote the use of volunteers and community groups for the provision of recreation programs, services, operation and maintenance and development of parks.
- 3.44 Develop a method of financing park and recreation facilities throughout the City using a variety of revenue and human resources.

### 3.4 AIR QUALITY

#### GOAL

To protect the health and welfare of Dinuba residents by promoting development that is compatible with air quality standards.



**OBJECTIVES**

- A. Develop consistent and accurate procedures for evaluating the air quality impacts of new projects.
- B. As part of the development review process, develop mitigation measures to minimize stationary and area source emissions.
- C. Develop transportation systems that minimize vehicle delay and air pollution.
- D. Develop consistent and accurate procedures for mitigating transportation emissions from new and existing projects.
- E. Encourage alternative modes of transportation including pedestrian, bicycle, and transit usage.
- F. Conserve energy and reduce air emissions by encouraging energy efficient building designs and transportation systems.

**POLICIES AND STANDARDS**

- 3.45 Coordinate with other local and regional jurisdictions, including the SJVAPCD and the California Air Resources Board (ARB), in the development of regional and county clean air plans and incorporate the relevant provisions of those plans into City planning and project review procedures. Also cooperate with the SJVAPCD and ARB in:
  - Enforcing the provisions of the California and Federal Clean Air Acts, state and regional policies, and established standards for air quality.
  - Economy clean fuel for city vehicle fleets, when feasible.
  - Developing consistent procedures for evaluating project-specific and cumulative air quality impacts of projects.
- 3.46 Require area and stationary source projects that generate significant amounts of air pollutants to incorporate air quality mitigation in their design, including:
  - The use of best available and economically feasible control technology for stationary industrial sources;
  - The use of EPA Phase II certified wood burning heater or pellet stoves in new residential units;

- The use of new and replacement fuel storage tanks at refueling stations that are clean fuel compatible, if technically and economically feasible; and
- The promotion of energy efficient designs, including provisions for solar access, building siting to maximize natural heating and cooling, and landscaping to aid passive cooling and to protect from winter winds.

- 3.47 Develop strategies to minimize the number and length of vehicle trips, which may include:
  - Promoting commercial/industrial project proponent sponsorship of van pools or club buses;
  - Encouraging commercial/industrial project day care and employee services at the employment site;
  - Encouraging the provision of transit, especially for employment-intensive uses of 200 or more employees; and
  - Providing expansion and improvement of public transportation services and facilities.
- 3.48 Encourage transportation alternatives to motor vehicles by developing infrastructure amenable to such alternatives by doing the following:
  - Consider right-of-way requirements for bike usage in the planning of new arterial and collector streets and in street improvement projects;
  - Require that new development be designed to promote pedestrian and bicycle access and circulation; and
  - Provide safe and secure bicycle parking facilities at major activity centers, such as public facilities, employment sites, and shopping and office centers.
- 3.49 Encourage land use development to be located and designed to conserve air quality and minimize direct and indirect emissions of air contaminants by doing the following:
  - Locate air pollution point sources, such as manufacturing and extracting facilities in areas designated for industrial development and separated from residential areas and sensitive receptors (e.g., homes, schools, and hospitals);

- Establish buffer zones (e.g., setbacks, landscaping) within residential and other sensitive receptor uses to separate those uses from highways, arterials, hazardous material locations and other sources of air pollution or odor;
- Consider the jobs/housing/balance relationship (i.e., the proximity of industrial and commercial uses to major residential areas) when making land use decisions;
- Provide for mixed-use development through land use and zoning to reduce the length and frequency of vehicle trips;
- Accommodate a portion of the projected population and economic growth of the City in areas having the potential for revitalization;
- Locate public facilities (libraries, parks, schools, community centers, etc.) with consideration of transit and other transportation opportunities;
- Encourage small neighborhood-serving commercial uses within or adjacent to residential neighborhoods when such areas are aesthetically compatible with adjacent areas; do not create conflicts with neighborhoods schools; minimize traffic, noise, and lighting impacts; encourage and accommodate pedestrian and bicycle access; and, are occupied by commercial uses that have a neighborhood-scale market area rather than a community-wide market area; and
- Encourage a development pattern that is contiguous with existing developed areas of the City.

## 4.0 URBAN BOUNDARY ELEMENT

### INTRODUCTION

Since it was originally incorporated in 1906 as a general law city, Dinuba has grown to an estimated 2007 population of approximately 20,002 persons. The General Plan includes an assumption of 3% population growth per year, to approximately 34,000 persons by the year 2026. The City's growth has been located within the present City boundary and not concentrated in one particular area. Future growth is primarily planned for the west and southern areas of town with a squaring off of the City limits to the north and east.

### PURPOSE OF THE URBAN BOUNDARY ELEMENT

The purpose of the Urban Boundary Element is to define the limits for extending City services and infrastructure so as to accommodate new development anticipated within the time frame of this General Plan. The element includes a 20-year Urban Development Boundary which is the City's ultimate physical boundary and service area for the next 20 years (2006-2026); a 10-year Urban Development Boundary which is the urbanizable area within which a full range of urban services will need to be extended to accommodate urban development; and a greenbelt to maintain separation between the Cutler-Orosi area, Reedley and Fresno County. Reference the Land Use map for the 10-and 20-year UDB's.

### SCOPE AND CONTENT OF THE URBAN BOUNDARY ELEMENT

The Urban Boundary Element is not a state-mandated element. It is, however, an important element because it limits leap-frog development and provides for an orderly transition from rural to urban land uses. The element comprises four sections: the Introduction; Purpose of the Urban Boundary Element; Scope and Content of the Urban Boundary Element; and Objectives, Policies and Standards. The Policies and Standards section establishes guidelines for new development over the next 20 years.

### 4.1 URBAN BOUNDARIES

#### OBJECTIVES

- A. Provide for an orderly and efficient transition from rural to urban land uses.
- B. Minimize urban sprawl and leap-frog development.
- C. Designate growth areas that can likely be served by foreseeable infrastructure improvements.

### POLICIES AND STANDARDS

- 4.1 First priority shall be given to development of vacant, underdeveloped, and/or redevelopable land where urban services are or can be made available. Parcels should be substantially contiguous to existing development.
- 4.2 Identify and use natural and man-made edges such as local roadways and waterways, as urban development limits for growth phasing lines.
- 4.3 Utilize low density residential land uses as a buffer and transition between long-term agricultural uses and urban development.
- 4.4 Prohibit the premature conversion of agricultural lands where agricultural preserves are present.
- 4.5 Encourage the use of parks and open space to enhance gateways to the City.

### 4.2 GROWTH POLICIES

#### OBJECTIVE

Implement growth policies which will guide the timing, type, and location of growth, preserve resource lands, protect natural features and open space, and encourage techniques which encourage energy conservation.

### POLICIES AND STANDARDS

- 4.6 Establish 10- and 20-year Urban Development Boundaries which include the City's ultimate physical boundary and service area for the next 20 years (2006-2026) plus an urban reserve and greenbelt area comprising an additional 30 percent.
- 4.7 Establish an Urban Development Boundary as the urbanizable area within which a full-range of urban services will need to be extended to accommodate urban development. This boundary shall be established based on the following factors:
  - a. Adequate residential, commercial and industrial capacity for the planning period.
  - b. Inclusion of a 30 percent vacancy factor ("flexibility factor") for residential and commercial development.
  - c. Provision of adequate industrial land.
  - d. Adequacy of infrastructure including existing and planned capacity of sewerage system, treatment plant, water system, schools, roadways, and other urban services and facilities.

*A city's Planning Area boundary may encompass incorporated and unincorporated territory bearing a relation to the city's planning. The Planning Area may extend beyond the Sphere of Influence. A city's Sphere of Influence, adopted by the LAFCo, encompasses incorporated and unincorporated territory that is the city's ultimate service area.*



e. Community growth priorities.

4.8 Maintain separation between Dinuba and the Cutler-Orosi Area, Reedley and Fresno County through the continuation and expansion of the City's greenbelt.

### **4.3 GROWTH MANAGEMENT COORDINATION**

#### **OBJECTIVE**

Coordinate growth management planning and implementation with the County.

#### **POLICIES AND STANDARDS**

4.9 Encourage Tulare County to strictly limit the establishment of new or expanded developments in the Urban Development Boundary.

4.10 Establish a Planning Area Boundary around the City consistent with Tulare County LAFCo's Guidelines. This area should represent a potential 50-year growth boundary with a greenbelt area. This area should include any "areas or communities of interest" that may affect the City.

## 5.0 URBAN DESIGN ELEMENT

### INTRODUCTION

The Urban Design Element is a guide to the future character of Dinuba as reflected in the use and appearance of new development and redevelopment. The guiding principles of the Urban Design Element encourage a more efficient, compact form of development in order to conserve land resources and create a more traditional neighborhood environment conducive to pedestrian activity. Older communities developed prior to World War II typically reflected particular design characteristics that have come to be known as “traditional neighborhood development.” Such designs include orienting household gathering spaces (porches, entryways) toward the front of the home; streets and sidewalks that accommodate pedestrians; neighborhood-oriented commercial development; and other designs that emphasize a unique sense of place.



Neighborhoods are the fundamental building blocks for community design in developing and improving residential areas. Schools, churches, and hospitals fit within the neighborhood and should be designed to blend seamlessly with other neighborhood components. Open space should also be thought of in the context of the neighborhood unit. Parks, drainage, and landscape should all be included in neighborhood specific plans. Furthermore, traffic, circulation, street design, bicycle networks and pedestrian routes to schools and other amenities should be thought of as components of the neighborhood.

The guiding principles of the Urban Design Element are:

- **Compact Development.** Providing access to what we need without having to use an automobile requires that destinations be in closer proximity. Development that concentrates services in village centers including homes, services, offices and public buildings contribute to more efficient growth patterns.
- **Mixed-Use Neighborhoods.** Mixed-use means locating a variety of different land uses -- housing, schools, small shops, offices and neighborhood services -- within walking distance of one another (or within the same structure in the downtown).

- **Pedestrian-Friendly Design.** People will walk, bicycle or take public transit to many activities, but only if it is pleasant, convenient and safe.
- **Provide a Comfortable and Interesting Environment.** Design attractive storefronts with windows and openings on the first floor, and provide amenities such as covered walkways, public plazas, benches, appropriate lighting and inviting places to eat and drink.
- **Create Streets for People as Well as Cars.** Streets and paths, arranged in a manner that provide pedestrians and bicyclists short, direct routes to their destinations, should connect all parts of a community.

### PURPOSE OF THE URBAN DESIGN ELEMENT

The Urban Design Element is intended to guide the development of the community consistent with the General Plan’s vision of new development that contains a variety of housing, parks, schools and neighborhood scale commercial uses. The element creates a sense of community with an emphasis on quality of life and livability while meeting the City of Dinuba’s expectations for high quality development.

The element establishes concepts, guidelines and standards for the Dinuba Planning Area to ensure consistency in the quality and character of development. The element contains goals and policies that are general in nature and may be interpreted with flexibility in their application to specific projects. It is hoped that these goals and policies will encourage the highest level of design quality while at the same time, provide the flexibility necessary to encourage creativity on the part of the project designer.

### SCOPE AND CONTENT OF THE URBAN DESIGN ELEMENT

The Urban Design Element contains goals, objectives, and policies to guide the scale and quality of development in Dinuba. Where appropriate, illustrations are provided as preferred examples of development guidelines. It is the intent of the Urban Design Element that the project developer or designer establish the architectural theme and style of each neighborhood or project with careful review and regulation by the City.

Major policy areas of the Urban Design Element are:

- Design Guidelines and Development Review
- Neighborhood Land Use Planning
- Single Family Residential
- Multifamily Residential
- Commercial
- The Downtown



- Neighborhood Entries
- Landscaping and Fencing

**GOAL**

It is the overall goal of the City to provide for the highest quality of development through the implementation of urban design policies and standards while allowing the private sector the design freedom to respond to market demand for size, character, theme and other variables of community design.

**5.1 DESIGN GUIDELINES AND DEVELOPMENT REVIEW**

**OBJECTIVE**

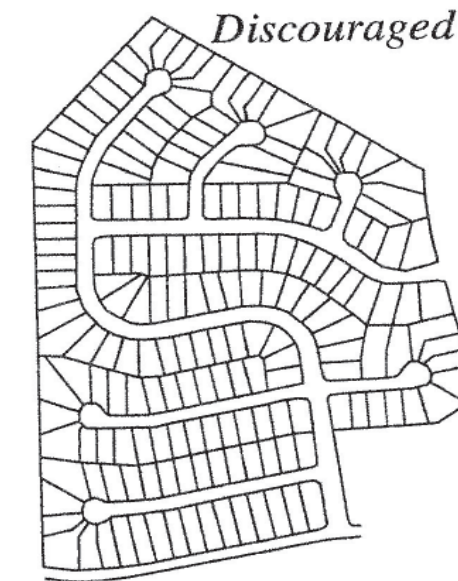
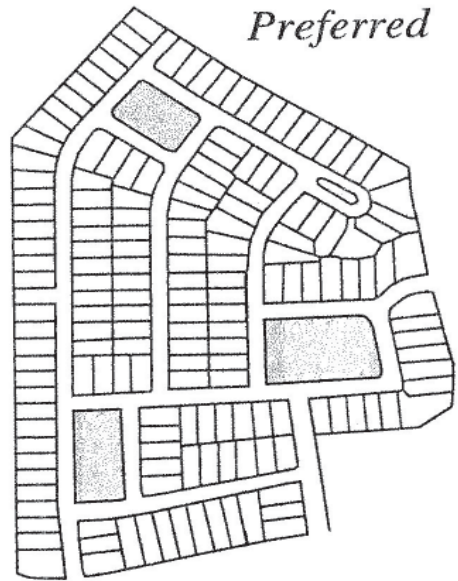
Develop design guidelines and a development review process wherein the City and the private sector work together to achieve the General Plan vision of overall community design.

**POLICIES AND STANDARDS**

- 5.1 Develop design review standards for structures, landscaping, and related development to facilitate compatibility with surrounding uses and the overall character of the community (Land Use Element policy No. 1.1).
- 5.2 The City shall establish a design review process to include the following:
  - a. The applicant shall consult with the City of Dinuba staff early in the process, before any design work is initiated, to verify the requirements of the appropriate General Plan policy and design requirements.
  - b. Upon completion of a preliminary site plan and/or architectural plans and elevations based upon General Plan policy and design requirements, the application may be submitted to the development department.
  - c. City staff will determine whether plans comply with the guidelines.

- 5.3 The developer shall submit architectural elevations, floor plans, and preliminary landscaping plans for all development types with exception of a building permit for an individual residential parcel. For single family subdivisions, multifamily projects, and commercial shopping centers, the developer shall submit standards outlining the architectural style, treatments, and materials consistent with the theme for the project demonstrating compliance with General Plan policies and standards.

**5.2 NEIGHBORHOOD LAND USE PLANNING**



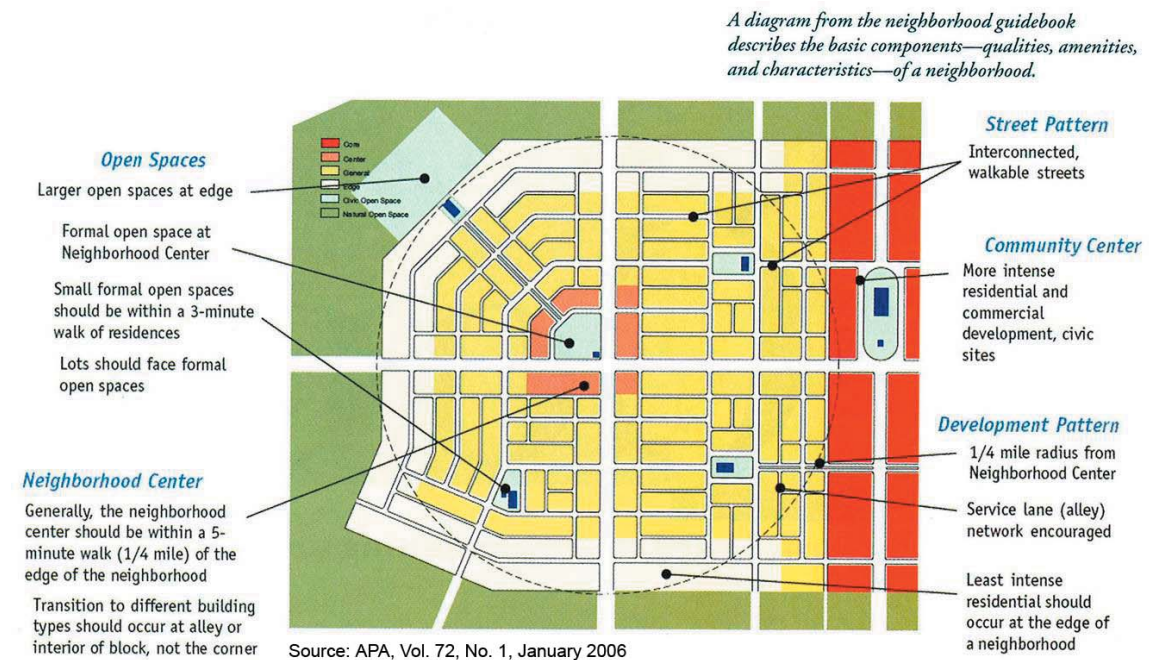
*This illustration contrasts a subdivision with good connectivity (top) and a subdivision with poor connectivity (bottom). The top subdivision offers multiple route choices while the lower subdivision requires circuitous trips to reach many of the parcels. The top subdivision also integrates park and open space in accessible, visible locations.*

**OBJECTIVE**

Create livable neighborhoods incorporating a sense of place and connectivity to other neighborhoods and the remainder of the City.

**POLICIES AND STANDARDS**

- 5.4 New residential development should be designed in easy walking and bicycling distance to neighborhood commercial areas and community facilities such as schools (a distance equal to approximately 1/4 mile). This guideline may require placement of new neighborhood-serving commercial centers within new subdivisions.
- 5.5 New residential subdivisions should be laid out in grid or modified grid pattern to create direct routes to surrounding developments and land uses. Major streets should be oriented when possible to capture views of the nearby Sierra Nevada. New subdivisions should limit long loop roads and cul-de-sacs, unless they are necessary to access parks or open space areas.
- 5.6 New residential subdivisions should provide strategically-placed parks that are visible and accessible from the front entries of the maximum number of homes. Parks should be used to define the form and shape of the residential subdivision rather than be “left over” parcels of land not available for development. The standard for neighborhood parks is a 3-5 acre park site in each neighborhood within a ¼ mile walking distance of all residents.





- 5.7 New residential subdivisions should provide an interconnected street system that allows for a hierarchy of transportation modes, including auto, pedestrian and bicycles with direct connections to neighborhood commercial centers, open space and recreation, other parts of the neighborhood, and adjacent districts and circulation routes. The emphasis should be on direct access and avoiding circuitous access from neighborhood to neighborhood.
- 5.8 In designing new streets, consideration should be given to traffic calming mechanisms, such as bulb-outs at intersections, strategically placed roundabouts or traffic circles, pedestrian refuges, and textured cross walks, among others (reference Table 2-1 in the Circulation Element for a list of traffic calming features).
- 5.9 Setbacks from the public street should be minimized to bring structures close to the street to encourage neighborhood interaction.
- 5.10 Street orientation must be considered for optimum energy efficiency, with respect to solar access. As many homes as possible should be oriented so that large areas of the roof and walls receive solar radiation from the south.
- 5.11 Utility services should be located underground.

**5.3 SINGLE FAMILY RESIDENTIAL**

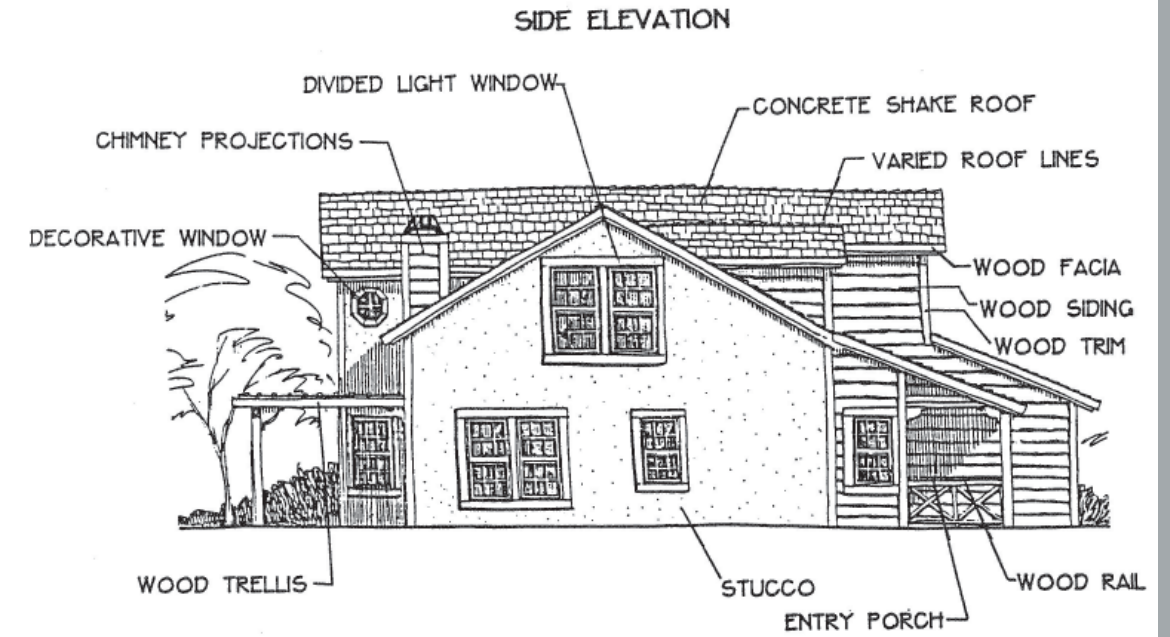
**OBJECTIVE**

Create single family neighborhoods incorporating layout and architectural styles that facilitate traditional neighborhood development techniques.



**POLICIES AND STANDARDS**

- 5.12 For single family dwellings, styles that reflect the architectural traditions and history of Dinuba and the San Joaquin Valley are preferred.
- 5.13 Good architectural design encourages neighborhood interaction and the ability to have “eyes on the street”. Streets with long expanses of blank garage doors should be avoided. Techniques to encourage good streetscapes include:
  - a. Locate the front doors of homes so that they are visible from the street.



- b. All dwellings should feature a useable front porch that dominates the facade of the unit. The City will consider allowing reduced front yard setbacks for units that feature a front porch.
  - c. Garages should be set back behind the front plane of the dwelling or should be set back and detached entirely.
  - d. Dwellings on corner lots should be designed with two “frontages”. Ideally, one street frontage will feature the front door, while the other street frontage features the garage and driveway.
- 5.14 Random setbacks of buildings and landscaping should be incorporated in all structural design and unit siting.
  - 5.15 Residential development should include a mix of one and two-story dwelling units wherever possible.
  - 5.16 The height and bulk of buildings should be appropriate to the size, shape and topography of the site and in harmony with its setting.
  - 5.17 Buildings should be designed to an approximate human scale and should not appear to be monumental or monotonous. The use of the following design elements will help in creating buildings properly scaled to people:
    - a. Breaking up building masses into smaller, staggered masses
    - b. Breaking up long wall surfaces and roof lines into discontinuous surfaces

- c. Randomly textured materials on roofs and walls
- d. Extended roof overhangs

5.18 Architectural styles and treatments should exhibit the following characteristics:

- a. Creates a complementary relationship with adjacent projects
- b. Creates architecturally distinct structures through use of various components
- c. Develops a compatible relationship between projects and buildings, and open space or recreation areas
- d. Avoids visual repetition, including discouraging “franchise architecture”.
- e. Maintains continuity within a project through use of similar architectural elements

5.19 The architectural styles and treatments selected for projects should utilize or incorporate some combination of the following features:

- a. Articulated facades
- b. Low plate lines
- c. Large overhangs
- d. Variated roof planes
- e. Recessed entries
- f. Greenhouses and skylights
- g. Balconies and broad porches
- h. Wainscoting
- i. Extensive windows

5.20 Materials used in the construction of residential and commercial structures should be selected from the following listing.

- a. Stucco and plaster

- b. Wood and dimensioned lumber
- c. Board on board
- d. Stone, rock, or brick
- e. Wood or wood replica shingles
- f. Slate -Metal or wood window dividers
- g. Wood railings
- h. Precast concrete or split-faced block (commercial)

5.21 Small-lot residential products, including condominiums, are encouraged in order to provide a variety of housing types, styles, and affordability. Small-lot single family alternatives include detached and attached units, zero lot line product, and “alley loaded” units adjacent to local or collector streets.

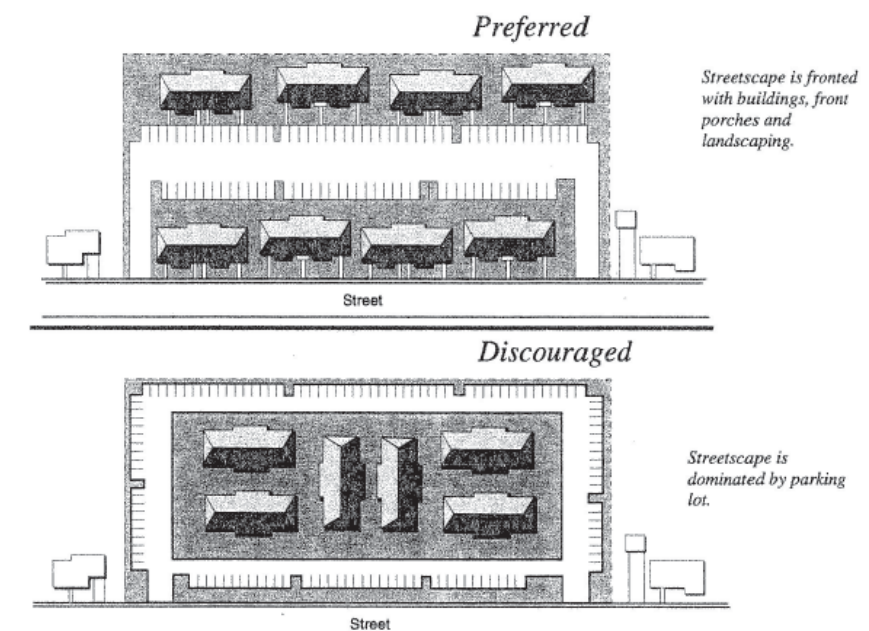
#### 5.4 MULTIFAMILY RESIDENTIAL

##### OBJECTIVE

Provide for multifamily housing that, regardless of size or number of units, are designed to integrate into the surrounding neighborhood.

##### POLICIES AND STANDARDS

5.22 Design techniques for multifamily projects include:



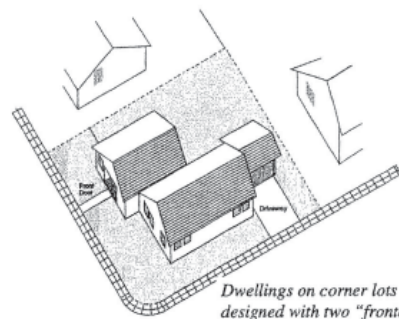
Encouraged

*This graphic shows the pleasing effects of a dwelling with a large, useable front porch. Doors and windows prominently face the street. The garage is set back well behind the front plane of the dwelling.*



Discouraged

*Streets with long expanses of blank garage doors are to be avoided.*



*Dwellings on corner lots should be designed with two “frontages” – a front door facing one street and garage facing the other.*



- a. Buildings should be “pulled forward” on the lot, towards the street, with parking to the rear. This helps multifamily projects better harmonize with the existing streetscape and surrounding neighborhood.
- b. Units fronting the street should include a front door facing the street.
- c. Units with doors facing streets should include a useable front porch.

5.23 Multifamily residential development should utilize architectural styles that are native and traditional to Dinuba and the San Joaquin Valley. These styles are addressed in the Single Family Residential policies and standards.

5.24 Visually harmonize multifamily residential development with adjacent residential neighborhoods by use of the following techniques:

- a. Integrate architectural elements and building articulation that is similar to the surrounding neighborhood.
- b. Use similar colors, details, and finish materials as those in the adjacent neighborhood.
- c. Use shape and massing that conforms to existing neighborhood scale.

5.25 Stairways should be designed as an integral part of the overall structure and should incorporate materials used in the main building. “Motel-style” prefabricated stairways composed of concrete and open wrought-iron railing is discouraged. Common stairways should be designed to serve a limited number of units.

5.26 Large roof mounted equipment is discouraged. Where necessitated, such equipment should be screened from view through the use of parapets or similar devices that complement the architectural character of the building.

## 5.5 COMMERCIAL

### OBJECTIVE

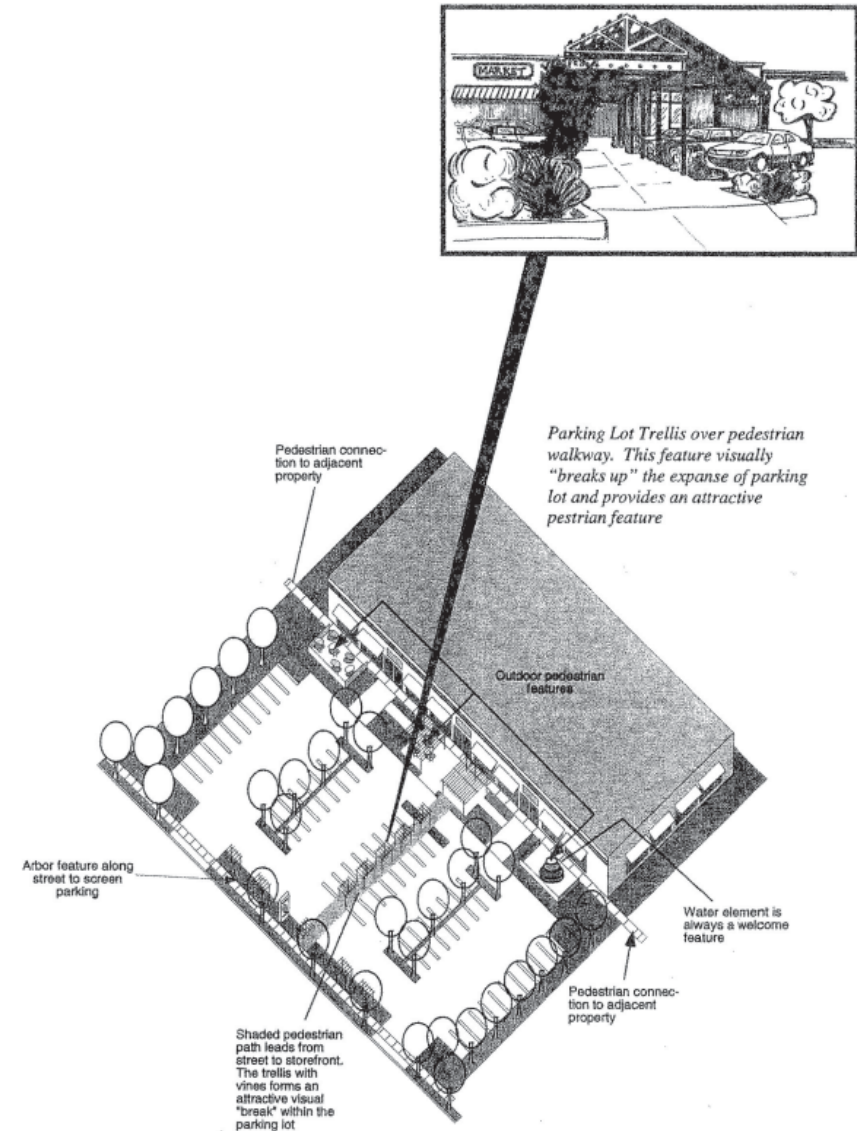
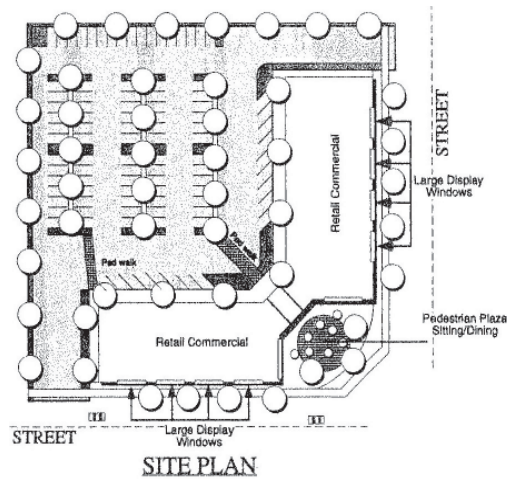
Provide for commercial development that is designed to integrate into the surrounding neighborhood.

### POLICIES AND STANDARDS

5.27 For neighborhood commercial projects, emphasis should be placed on

pedestrian oriented design features, rather than design that is solely automobile-oriented.

5.28 The preference is that new commercial integrate well into surrounding neighborhoods. Towards this objective, new neighborhood commercial development should be designed in a “village core” or “main street” fashion, with buildings set at the back of sidewalk, and parking to the rear or to the side.

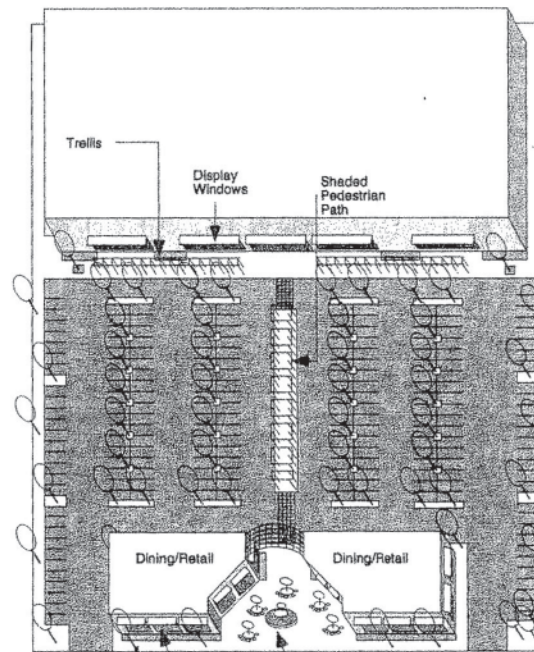
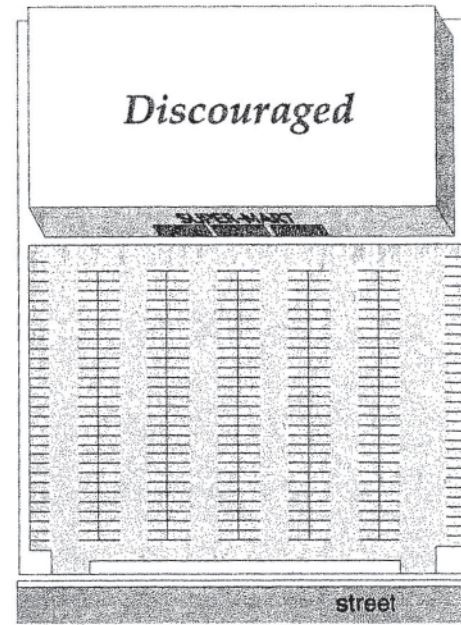


5.29 Encourage two-story mixed use development in commercial centers, such as office over retail.

5.30 Pedestrian access between individual commercial buildings and adjacent land uses should be emphasized through the use of generous walkways and landscaping.



- 5.31 Courtyard spaces that provide shading and opportunities for outdoor seating are encouraged for all neighborhood commercial centers.
- 5.32 Long expanses of blank, featureless walls should be avoided. Long building facades should be broken up with offsets, cornices, turrets, cupolas or other design elements.
- 5.33 Relatively large scale shopping centers and “big box” type developments have unique development requirements. On project sites larger than 15 acres, the following policies shall apply in addition to other policies within the General Plan:



*Additional buildings are placed at the street edge to visually screen an otherwise expansive parking lot. An outdoor plaza leads to a pedestrian arbor through the parking lot to the main building.*

- a. Where practical, locate pad buildings closer to major streets to provide a strong visual and pedestrian relationship to the street and locate some parking and service functions behind the building(s).
- b. Provide a raised landscaped median at major entrances to separate ingress and egress lanes. Incorporate prominent entry features, vertical landscape forms and/or seasonal color at both vehicular and pedestrian project entrances.
- c. Locate drive-through lanes away from adjoining residential uses. Locate speakers and menu boards so that noise is

not directed toward residential uses and incorporate a screen wall and landscaping to mitigate noise. Utilize architectural features, screen walls, landscaping and canopies to integrate drive-throughs into the overall building design.

- d. In large multi-building projects, organize the site layout to provide functional pedestrian spaces, plazas and amenities between or in front of buildings.
  - e. Design sites to minimize pedestrian and vehicular conflicts. Where pedestrian paths cross vehicular drives, provide paving materials, textures or colors to emphasize the conflict point. Use an outer drive aisle to move traffic away from the aisle or aisles in front of store areas.
  - f. Provide convenient bicycle parking in locations that do not interfere with pedestrian circulation. Disperse bicycle parking facilities throughout larger sites and locate them in convenient and visible areas.
  - g. Allow for outdoor dining and/or other amenities to enliven plazas and open space areas. Outdoor dining and pedestrian amenities should be separated or screened from residential areas and from traffic.
  - h. Minimize driveway cuts on major streets by providing cross-access easements and shared access driveways between adjacent commercial projects.
  - i. In large multi-building projects, vary the size, massing and height of the buildings in relation to each other.
  - j. The use of bright color schemes should be justified by the overall design, and may not be appropriate in many contexts.
  - k. Buildings should have clearly defined customer entrance(s) incorporating elements such as canopies, overhangs, arches, entrance framed by outdoor pedestrian features or enhanced landscaping, or architectural details such as tile work and moldings to frame the entryway.
- 5.34 For commercial shopping centers, the following general principles should be followed:
- a. Avoid large expanses of parking lot. Parking areas should be screened with smaller buildings set forward toward the street
  - b. Establish pedestrian plazas and walkways.

- c. Use trellises and shade trees along walkways and in parking lots.
  - d. Efforts should be made to establish pedestrian connections with adjacent commercial areas and surrounding residential neighborhoods.
  - e. To the maximum extent feasible, situate buildings against the street with parking to the rear.
  - f. To the extent practical, buildings should feature large display windows and entrances along the street sidewalk.
- 5.35 As many pedestrian amenities should be included in the parking lot and building exterior as possible, including:
- a. Shade trees and landscaping.
  - b. Parking lot pathways, shaded with trellises.
  - c. Store front pedestrian areas with seating fountains and statuary.
  - d. Effective pedestrian connections with adjoining properties.
- 5.36 Architectural styles that reflect the traditions and history of Dinuba and the San Joaquin Valley are preferred over “modern” “contemporary” and franchise design styles that are repeated in every other community.
- 5.37 Buildings should be articulated in a fashion similar to the pedestrian-oriented buildings found in downtown Dinuba. Use of insets, overhangs or arcades (roofed passageways), cupolas or clock towers, trellises and other design elements are encouraged. Building facades should avoid blank walls greater than 30 feet in length at street level.
- 5.38 Buildings should feature large show-windows along sidewalks and along street frontages.
- 5.39 Signs should be integrated into the design of the building and complement the overall architectural style.
- 5.40 Stark color contrasts in signs should be avoided. Sign colors should be muted - use of significant quantities of primary colors should be avoided.
- 5.41 Where free-standing signs are proposed, monument signs are preferred over pole signs.
- 5.42 The identification and directional signs including the location, materials, colors, copy and the method of signing, size, and construction shall be

approved by the City in accordance with the existing Sign Ordinance.

5.43 All parking lot and driveway lighting should provide uniform illumination. Accent illumination is recommended at key points such as entrances, exits, loading zones, and drives.

5.44 Lighting should be shielded and situated so as to not cause glare or excessive light spillage on neighboring sites.

## 5.6 THE DOWNTOWN

### OBJECTIVE

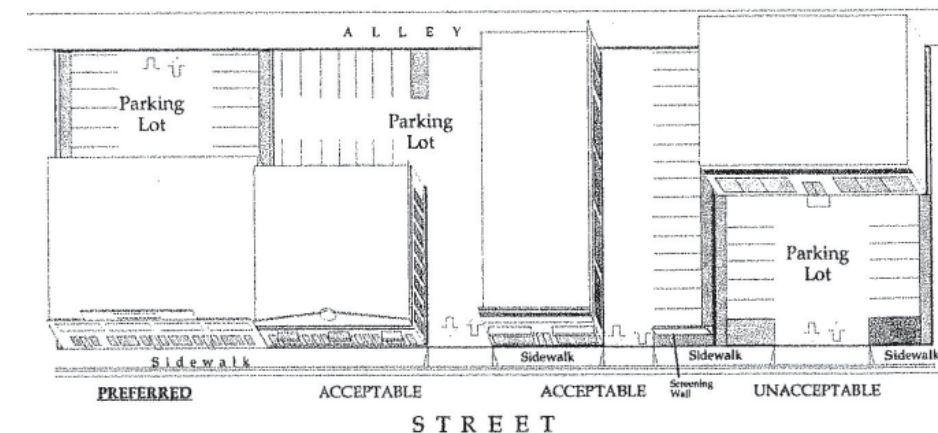
New development in the downtown should reinforce the area’s strong pedestrian-oriented shopping environment.



### POLICIES AND STANDARDS

5.45 New buildings should be located along the front property line, at the back of the sidewalk. On-site parking should be located to the rear or to the side of buildings.

*Relationship of Buildings to the Street*

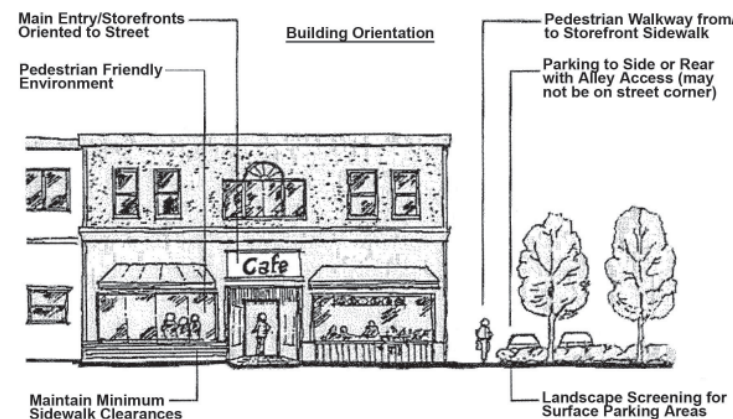


5.46 Useable open spaces such as courtyards and plazas with outdoor seating, landscaping, water features, etc. are encouraged. Pedestrian corridor access (paseos) should be provided to link rear parking lots to the street.



- 5.47 The following urban design elements are considered most desirable for new downtown development or redevelopment:
  - a. Significant wall articulation (e.g. insets, canopies, trellises or other ornamentation).
  - b. A variety of surface textures, provided that they are appropriate to the particular architectural style of the building.
  - c. Large display windows at street level for the display of merchandise and to allow shoppers to see inside the store.
  - d. Overhangs and arcades.
  - e. Regular window placement.
  - f. Pedestrian-scale signs that compliment the style and character of the individual building.
- 5.48 The scale of a large building should be “broken up” or reduced by creating a horizontal emphasis of the building. This can be accomplished through the proper use of window patterns, roof overhangs, the use of trim moldings, awnings, eaves, or other ornamentation, or by using a combination of complimentary colors and through the use of landscaping.
- 5.49 Blank solid walls of buildings visible from public view, including bland areas above cantilevered canopies should be avoided. If such walls are necessary for interior or structural reasons, the structure wall shall be treated with some form of articulation such as larger awnings, cornice bands, wainscoting or the like.
- 5.50 The facades of adjacent structures should be considered in the design of new buildings to avoid clashes in architectural style and materials.

5.51 Canopies and awnings are desirable elements in the downtown and are encouraged to shelter the openings of each building from sun and rain. New canopies and awnings should be



- respective of the style and character of the structure on which they are located, particularly in terms of materials and colors.
- 5.52 Awnings, trellises and other accessory structures that do not restrict pedestrian or vehicular movement may project into the front right-of-way.
- 5.53 Maintain existing historically significant buildings when possible. Any physical changes shall be done in a manner that is consistent with the original architectural style.
- 5.54 Mechanical equipment (e.g. air conditioning units) should be screened from view from the public right of way through the use of parapets, cornices or other treatments. Mechanical equipment should not be mounted on street arcades or other visible locations.
- 5.55 Every building should be designed with a precise concept for signage. Provisions for the placement of signs, the scale of signage in relation to the building, and sign readability should be considered in developing the sign concept. All signage should be compatible with the building and site design relative to color, materials, and placement.
- 5.56 Pedestrian-oriented “projecting” signs are encouraged. This type of sign should be at least eight feet above the sidewalk. Projecting signs should be used in lieu of a flush-mounted wall sign.
- 5.57 Street furnishings such as benches, fountains and public art should be incorporated at appropriate locations throughout the downtown.

## 5.7 NEIGHBORHOOD ENTRIES

### OBJECTIVE

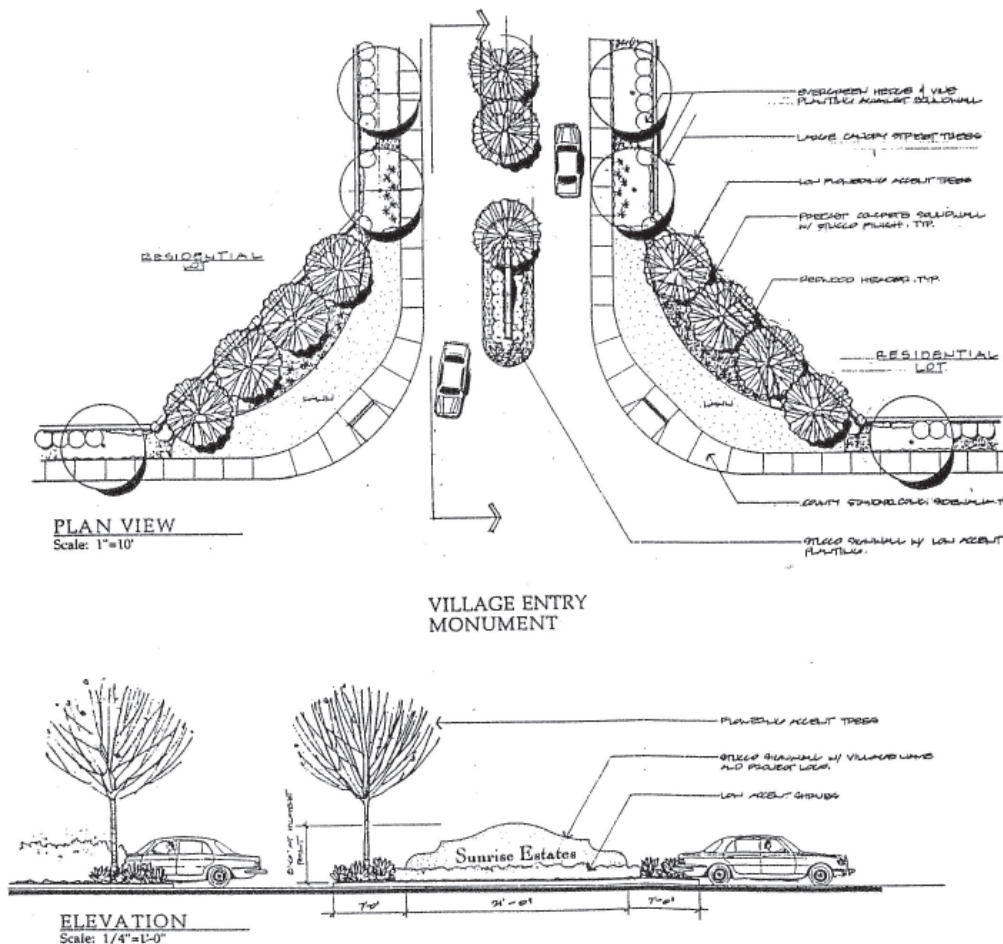
Distinctive community character shall be defined through the use of entry points into neighborhoods. Monumentation at major intersections and overall wall treatment creates definition and provides a sense of belonging and direction.

### POLICIES AND STANDARDS

- 5.58 Neighborhood entry monumentation occurs at one or two corners of the entry point to residential neighborhoods. These entries provide a reference point for both the resident as well as the visitor, and the design allows for flexibility and interpretation based on the individual developer’s needs. The criteria include:
  - a. Landscape / Hardscape from edge of sidewalk to back of property line



- b. Freestanding project monument sign
- c. Grove of small, accent narrowing trees offset to side



- d. Accent shrubs and ground cover from sidewalk to neighborhood wall
- e. Turf area with boulders and landscape mounds extending out from landscaped area and parallel to sidewalk

5.59 All walls adjoining roadway landscaping shall be located entirely within the landscaping parcel (outside of street right-of-way), allowing for common maintenance by a landscaping and lighting district. Such walls shall be termed “community walls” and exclude residential rear yard and side yards not adjoining a public street or common area, and single family front yard enclosure fencing.

## 5.8 LANDSCAPING AND FENCING

### OBJECTIVE

Public and private development shall create a cohesive, complimentary landscape design.

### POLICIES AND STANDARDS

- 5.60 Landscaping should be designed to establish project identity and to accentuate community values.
- 5.61 Develop a city-wide street tree and landscape master plan to delineate neighborhoods, master and specific plan areas (Land Use Element policy No. 1.4).
- 5.62 New residential development abutting an arterial or collector will be encouraged to use a berm/swale with landscaping instead of a masonry wall between the landscaping strip, sidewalk and maintenance district setback. A masonry wall may be used behind the berm/swale or the berm/swale may go half-way up the masonry wall. This will limit the view of the wall from the street and still provide privacy to residents (Land Use Element policy No. 1.12).
- 5.63 Multifamily developments should have a highly landscaped appearance, ideally incorporating an appropriate combination of turf, shrubs and shade trees.
- 5.64 Single family residential development shall include front yard landscaping installed by the developer.
- 5.65 Each residential lot shall receive a minimum of one street tree, fifteen (15) gallon planter, planted in the parkway or right-of-way. Corner lots shall receive a minimum of two trees, fifteen (15) gallon container, planted in the parkway or right-of-way. Tree variety shall be chosen from the Street Tree Master Plan.
- 5.66 Existing mature trees should be incorporated into new development when feasible.
- 5.67 Utility services and enclosures shall be screened from view from streets and adjacent properties with landscape materials consistent with the neighborhood palette.
- 5.68 Whenever possible, plant material should be designed to shade the western exposure of residential structures.
- 5.69 Commercial landscaping should incorporate a combination of shrubs, turf, trees and flowers. Where increased maintenance allows, additional elements such as benches, fountains and statuary should be included. Landscaping should be designed to accentuate positive design elements

- and screen negative views. Landscaping should be designed to ensure safety and visibility.
- 5.70 All areas of commercial sites not occupied by structures or paving shall be landscaped with ground cover or plant materials.
- 5.71 Vines and climbing plants integrated upon buildings, trellises and perimeter garden walls are strongly encouraged. Some plants that are encouraged include bougainvillea, grape ivy, and wisteria vines. Care should be taken that vines that might damage masonry walls are not used.
- 5.72 Parking lots shall feature landscaping to promote an attractive visual environment and reduce summer heat buildup. The following techniques are recommended:
- Landscaping or mounding shall be provided in the area between a parking lot and the street right of way. Alternately, a low screening wall with climbing vegetation shall be provided. For each four spaces, one shade tree shall be provided. Shade trees shall be provided to obtain shading of 50% shading of the parking lot within fifteen years.
  - Landscaping should be situated so that it does not interfere with vehicle sightlines, nor with the front end of parked vehicles.
  - Landscape planters should be provided along masonry walls and along building walls. Climbing vines should be established to screen masonry walls and prevent graffiti.
- 5.73 Informal groupings of street trees spaced at an average of thirty feet (30') on center are required along commercial frontages.
- 5.74 Evergreen background and/or deciduous, accent grove trees should be integrated around commercial sites to provide visual windows into the commercial project should be created by massing grove trees.
- 5.75 Fencing should be considered an integral part of the architectural character of a project.
- 5.76 Where chain link fencing is used, the fence should be finished in gloss vinyl black or green - as opposed to unfinished galvanized metal.
- 5.77 Walls and fences shall be constructed of materials, colors, and textures similar and harmonious with the architecture. Walls and fences may be offset occasionally to avoid visual monotony. Variety in materials, design and height is encouraged.
- 5.78 Wood fencing is permitted within individual neighborhoods provided the fencing is not readily visible from arterials and collectors, except as located behind the front yard.
- 5.79 No fence or wall shall exceed six feet in height except as may be required in limited instances for noise attenuation or privacy. Any fences or wall in the front setback area shall not exceed four feet in height from grade.
- 5.80 All fences and walls shall be designed and constructed as part of the overall architectural and site design. All materials shall be durable and finished in textures and colors complimentary of the overall architectural design. Permitted materials: Stone veneer, stucco (including stucco covered block), masonry, brick, slump block, block and wrought iron combination, and wood. Prohibited materials: Barbed wire, wire, electrically charged fences, plain exposed concrete block, plastic materials, corrugated metal.



## 6.0 NOISE ELEMENT

### INTRODUCTION

Noise levels within the city of Dinuba affect the quality of life of people living and working in the City. The most significant noise levels within the community are associated with the railroad and roadways. High noise levels associated with these and other activities can create stress and irritation. The Noise Element addresses the physiological, psychological and economic effects of noise by providing effective strategies to reduce excessive noise and limit community exposure to loud noise sources.

### PURPOSE OF THE NOISE ELEMENT

Government Code § 65302 (F) states that a City's General Plan must include "A noise element which shall identify and appraise noise problems in the community. The noise element shall recognize the guidelines established by the Office of Noise Control in the State Department of Health Services and shall analyze and quantify, to the extent practicable, as determined by the legislative body, current and projected noise levels for all of the following sources:

1. Highways and freeways.
2. Primary arterials and major local streets.
3. Passenger and freight on-line railroad operations and ground rapid transit systems.
4. Commercial, general aviation, heliport, helistop, and military airport operations, aircraft overflights, jet engine test stands, and all other ground facilities and maintenance functions related to airport operation.
5. Local industrial plants, including, but not limited to, railroad classification yards.
6. Other ground stationary noise sources identified by local agencies as contributing to the community noise environment."

### SCOPE AND CONTENT OF THE NOISE ELEMENT

The State of California recognizes the relationship between noise and noise-sensitive uses and has adopted State Guidelines for Noise Elements. This Noise Element satisfies the requirements of State planning law and is a mandated component of the General Plan. Government Code § 65302 (f) establishes the required components of the Noise Element. The Element also complies with California Health and Safety Code Section 56050.1 guidelines for Noise Elements.

Future noise conditions from short- and long-term growth are quantified and identified as noise exposure contours. This noise information serves as the basis for developing guidelines for identifying compatible land uses; identifying the proper distribution of land uses on the General Plan Land Use Map; and establishing proper development standards.

The Noise Element comprises four sections: the Introduction; Purpose of the Noise Element; Scope and Content of the Noise Element; and the Goals, Objectives and Policies. In the Goals, Objectives, and Policies section, major issues pertaining to noise sources are identified and related policies are established. The objectives are statements of the City's desires and comprise broad statements of purpose and direction. The policies and standards serve as guides for reducing or avoiding adverse noise impacts on the population.



Figure 6-1 shows the decibel levels associated with different common sounds, and illustrates typical sound levels, Figure 6-2 provides noise level criteria for a variety of land uses, and Figure 6-3 illustrates the reduction in sound from a solid barrier.

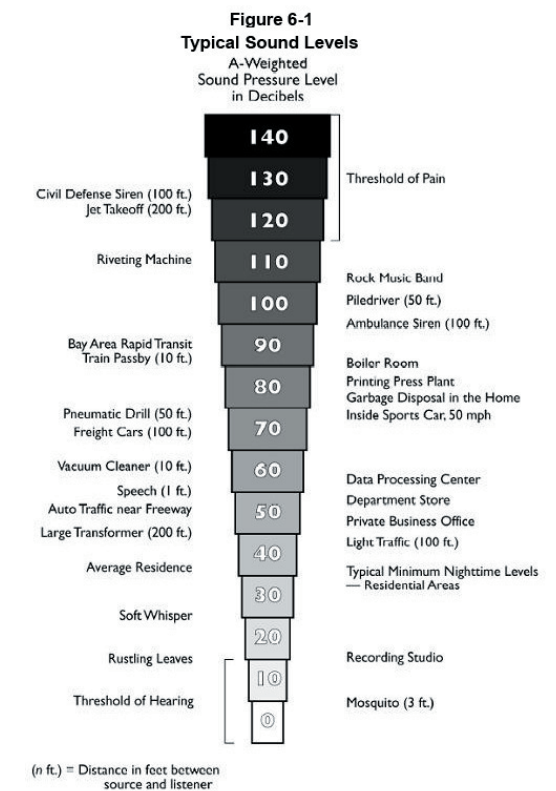
Sound generally dissipates at a rate of 3 to 6 dBA per doubling of distance from a source within 200 to 300 feet of that source. Its decay rate beyond that is highly variable depending on the atmospheric (mainly temperature variations, wind currents, and humidity) and terrain conditions between the source and listener. However, sound levels generally decrease with increasing distance from a source. For additional information consult the General Plan Background Report.

### GOALS

1. Minimize the exposure of Dinuba residents to excessive noise.
2. Reduce noise levels from traffic and railroad operations.

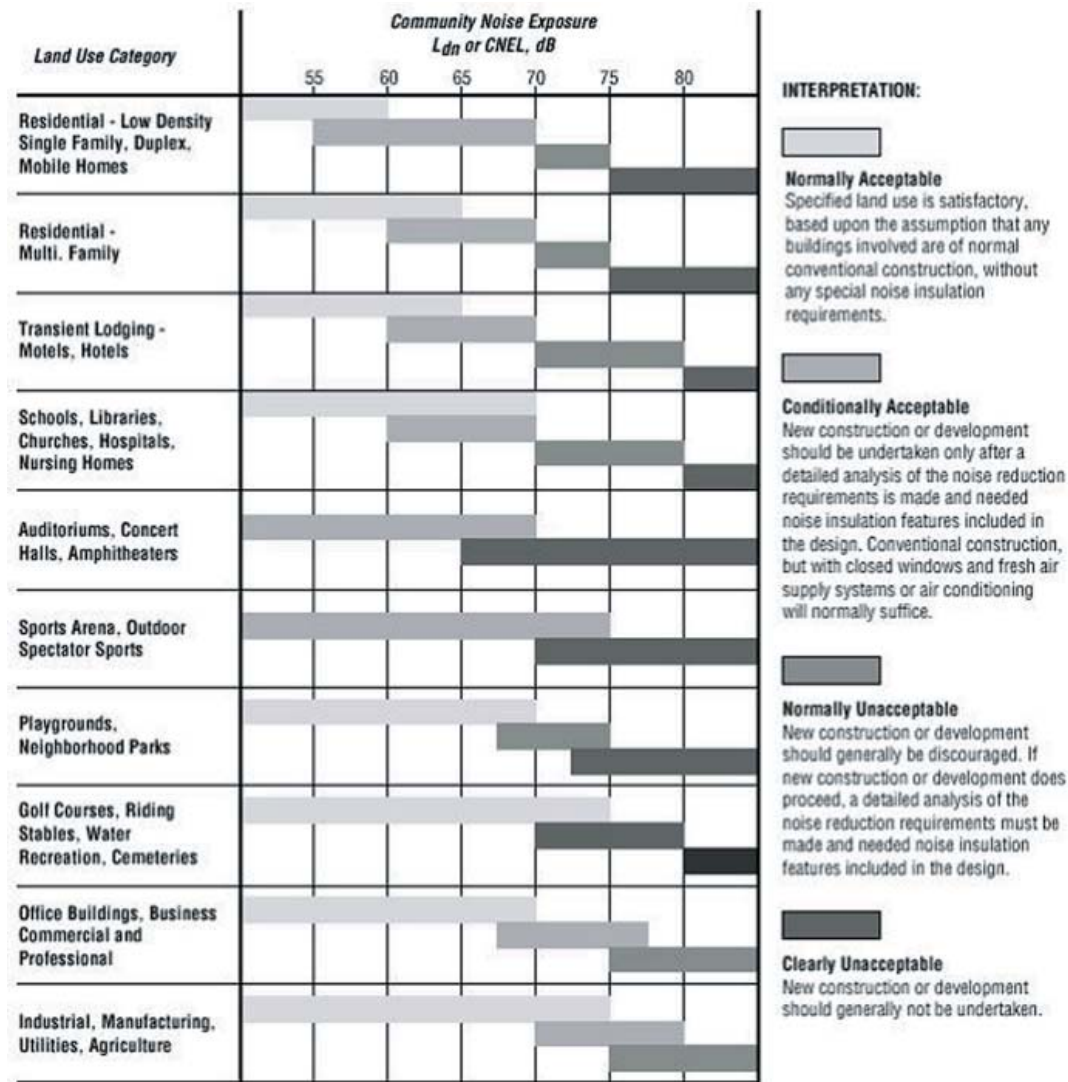
### OBJECTIVES

- A. Dinuba shall remain committed to preserving the community's noise environment.



*Community Noise Equivalent Level (CNEL), is the 24 hour energy equivalent level derived from a variety of single-noise events, with weighting factors of 5 and 10 dBA applied to the evening (7 p.m. to 10 p.m.) and nighttime (10 p.m. to 7 a.m.) periods to allow for greater sensitivity to noise during these hours. Ldn is the Day-Night Average Level. The average equivalent A-weighted sound level during a 24-hour day, obtained after the addition of 10 decibels to sound levels in the night after 10 p.m. and before 7 a.m.*

Figure 6-2

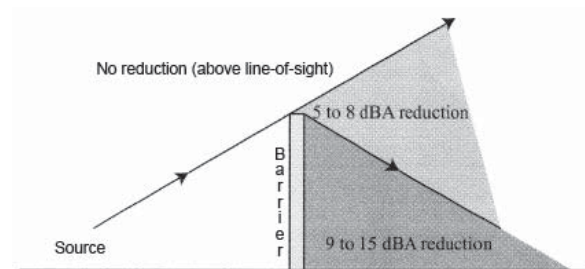


Source: State of California, General Plan Guidelines 2003, page 250

B. To preserve the tranquility of residential areas by preventing noise producing uses from encroaching upon existing or planned noise-sensitive uses.

Figure 6-3

C. To emphasize the reduction of noise impacts through careful site planning and project design, giving second preference to the use of noise barriers and/or structural features to buildings containing noise-sensitive land uses.



Source: Time-Saver Standards for Urban Design, McGraw-Hill, 2003, page 7.8-4

D. To educate the citizens of the City concerning the effects of exposure to excessive noise and the methods available for minimizing such exposure.

## POLICIES AND STANDARDS

- It shall be deemed unlawful for any devices, appliances, equipment or vehicles on public or private property abutting noise sensitive land uses to operate between the weekday hours of 7:00 p.m and 6:00 a.m. and between the weekend hours of 7:00 p.m. and 9:00 a.m.
- The City of Dinuba will update its Noise Ordinance (Title 8, Health and Safety, Chapter 8-36) to the following standards with regards to interior and exterior noise standards:

Exterior Noise Standards - Fixed Noise Level Standards, dBA			Residential Interior Noise Standards Noise Level Standards, dBA		
Cumulative Number of minutes in any one-hour time period	Daytime 6 a.m. to 7 p.m.	Evening and Nighttime 7 p.m. to 6 a.m.	Cumulative Number of minutes in any one-hour time period	Daytime 6 a.m. to 7 p.m.	Evening and Nighttime 7 p.m. to 6 a.m.
30	50	45	5	45	35
15	55	50	1	50	40
5	60	55	0	55	45
1	65	60			
0	70	65			

- The City will utilize the noise/land use compatibility standards in Figure 6-2 as a guide for future planning and development decisions.
- Areas within Dinuba shall be recognized as noise impacted if exposed to existing or projected future noise levels at the exterior of buildings exceeds 65 dB L<sub>dn</sub> (or CNEL).
- Noise sensitive land uses shall be discouraged in noise impacted areas unless effective mitigation measures are incorporated into the specific design of such projects to reduce exterior noise levels to 65 dB L<sub>dn</sub> (or CNEL) or less and 45 dB L<sub>dn</sub> (or CNEL) or less within interior living spaces.
- Industrial, commercial or other noise generating land uses (including roadways, railroads, and airports) should be discouraged if resulting noise levels will exceed 65 dB L<sub>dn</sub> (or CNEL) at the boundary areas of planned or zoned noise sensitive land uses.
- The City shall enforce applicable State Noise Insulation Standards (California Administrative Code, Title 24) and Uniform Building Code (UBC) noise requirements.
- New equipment and vehicles purchased by the City should comply with noise level performance standards consistent with the best available noise reduction technology.



6.9 The preferred method of noise control used is thoughtful site design. Secondly, noise control should be achieved through the use of artificial noise barriers. Site and building design guidelines may include:

- a. Noise sensitive land uses should not front onto the primary noise source. Where this is not possible, the narrow portion of the building should face the primary noise source, and the interior layout should locate the most sensitive areas away from the noise source by placing garages, storage facilities, carports or other such areas nearest the noise source.
- b. Site design should permit noise to pass around or through a development. This can be achieved by placing the narrow or convex portion of the structure toward the primary noise source.
- c. Commercial and industrial structures should be designed so that any noise generated from the interior of the building is focused away from noise sensitive land uses.
- d. Two story residential construction should be avoided, where possible, immediately adjacent to arterials or collectors unless an adequate combination of noise attenuation procedures are used.
- e. When possible, residential cul de sacs should be perpendicular to adjacent arterials or collectors.
- f. Loading and unloading activities for commercial uses should be conducted in an enclosed loading dock, preferably with a positive seal between the loading dock and trucks.

6.10 The City shall review all relevant development plans, programs and proposals to ensure their conformance with the policy framework outlined in this Noise Element.

6.11 Prior to the approval of a proposed development in a noise impacted area, or the development of an industrial, commercial or other noise generating land use in or near an area containing existing or planned noise sensitive land uses, an acoustical analysis may be required if all of the following findings are made:

- a. The existing or projected future noise exposure at the exterior of buildings which will contain noise sensitive uses or within proposed outdoor activity areas (patios, decks, backyards, pool areas, recreation areas, etc.) exceeds 65 dB  $L_{dn}$  (or CNEL).
- b. Interior residential noise levels resulting from offsite noise are estimated to exceed 45 dBA.

c. Estimated or projected noise levels cannot be reduced to the noise exposure limitations specified in this Noise Element by the application of Standard Noise Reduction Methods.

When noise studies are necessary they should:

- a. Be the responsibility of the applicant.
- b. Be prepared by an individual or firm with demonstrable experience in the fields of environmental noise assessment and architectural acoustics.
- c. Include representative noise level measurements with sufficient sampling periods and locations to adequately describe local conditions.
- d. Include estimated noise levels in terms of dB  $L_{dn}$  (or CNEL) for existing and projected future (10-30 year hence) conditions, with a comparison made to the adopted policies of the Noise Element.
- e. Include recommendations for appropriate mitigation measures to achieve compliance with the adopted policies and standards of the Noise Element.
- f. Include estimates of noise exposure after the prescribed mitigation measures have been implemented. If compliance with the adopted standards and policies of the Noise Element will not be achieved, a rationale for acceptance of the project must be provided.
- g. The acoustical analysis should be prepared as early in the project review or permitting process as possible so that noise mitigation measures may be an integral part of the project design rather than an afterthought.

6.12 The City will seek to reduce impacts from ground borne vibrations associated with rail operations by requiring that habitable buildings are sited at least 100-feet from the center-line of the tracks, whenever feasible.

6.13 Encourage the Union Pacific railroad to maintain existing track in good condition and minimize train horn soundings to the extent allowed by law.

6.14 Require development of habitable buildings within 100-feet from the centerline of the railroad tracks to provide a study demonstrating that ground borne vibration issues associated with rail operations have been adequately addressed (i.e., through building siting or construction techniques).

## 7.0 PUBLIC SERVICES AND FACILITIES ELEMENT

### INTRODUCTION

The Public Services and Facilities Element addresses the community need for public services and facilities. The City is currently well served with infrastructure, and with master plans in place to guide capital spending to make improvements as necessary. Future development of the remaining vacant land within the City will require expansion of public services and facilities to meet the increase in demand for service. Planning for this future increase in demand will ensure that the needs of future residents for public services and infrastructure are met, while avoiding adverse impacts to the existing community.

### PURPOSE OF THE PUBLIC SERVICES AND FACILITIES ELEMENT

The purpose of the Public Services & Facilities Element is to ensure that sufficient levels of public services are provided as Dinuba develops. Working in conjunction with the Land Use Element, the Public Services & Facilities Element plans for the needed expansion and funding of public services and infrastructure to coincide with new development.

### SCOPE AND CONTENT OF THE PUBLIC SERVICES AND FACILITIES ELEMENT

The Public Services & Facilities Element is not a state-mandated element, however, the issues addressed within this Element closely relate to the Land Use Element. The Element is comprised of four sections: the Introduction; Purpose of the Public Services & Facilities Element; Scope and Content of the Public Services & Facilities Element; and the Objectives, Policies and Standards. In the Objectives, Policies and Standards section, major issues related to the provision of public services and facilities are identified and related policies and standards are established to address these issues. The objectives, which are overall statements of the community's desires, are comprised of broad statements of purpose and direction. The policies and standards serve as guides for infrastructure and facility improvements to provide sufficient levels of service.

### 7.1 WASTEWATER TREATMENT PLANT, SANITARY SEWER, STORM DRAINAGE

#### OBJECTIVE

Coordinate facilities and services planning to implement land use goals and objectives through the development of facilities and services.

#### POLICIES AND STANDARDS

7.1 Continue to coordinate community irrigation ditch issues with Alta Irri-

gation District, private ditch companies, private land owners, and public agencies. Require that irrigation ditches be piped prior to development on adjacent property.

- 7.2 Developers shall prepare an infrastructure and public services assessment as part of the annexation application to determine infrastructure needs, feasibility, timing, and financing. It is the policy of the City that new growth shall pay its own way.
- 7.3 Prepare and implement City-wide infrastructure master plans which implement adopted land use goals, objectives and policies and federal and State regulations. These master plans shall be implemented through various funding mechanisms including assessment district, property owner's associations user fees, development impact fees, mitigation payments, reimbursement agreements and/or other mechanisms which provide for equitable distribution of development and maintenance costs.
- 7.4 Require the development and extension of infrastructure to proposed developments according to adopted elements and master plans. The City shall use reimbursement agreements or other financing techniques to reimburse developers for an oversizing cost which may be required. Projects, which are not contiguous to existing urban development, shall be required to assess the cumulative impact of all non-contiguous development.
- 7.5 Coordinate urban growth management planning with public and private utilities.
- 7.6 Design runoff drainage structures to decrease erosion.
- 7.7 Urban development in floodway areas shall be in accordance with regulations of the Federal Emergency Management Agency.
- 7.8 All existing developments shall eventually convert to City sewer and water systems. Require all new residential development within the City limits to hook up to City sewer.



*A retention basin is used to contain storm water or rain runoff. A retention basin provides an area to hold water from a small surrounding drainage area that would otherwise flow into other areas. The water remains in the local area that it was deposited in. This is opposed to a detention basin that holds water for a limited period of time from a larger basin area to prevent flooding and releases all the water contained in a short period of time.*



- 7.9 Development fee credit may be given but shall not exceed the amount of fees.
- 7.10 Developers shall construct all tributary facilities necessary to connect to major facilities, whether or not the major facilities have yet been constructed.
- 7.11 Temporary drainage facilities may be constructed by the developer if the major facilities are not available, subject to City determination and approval. The developer will also be required to pay all applicable drainage fees in addition to constructing temporary facilities at his/her own cost.
- 7.12 Temporary drainage facilities (ponding basins) shall be dedicated to the City, with a reversionary clause which specifies that if the basin is abandoned within ten years, the property would revert to the original owner, subject to redevelopment of the site satisfactory to the City.
- 7.13 Upon the collection of adequate funds, the City will install major facilities in accordance with the master plan at the locations deemed most essential by the City, with due regard for community needs and areas from which fees were collected. To make the best use of funds, growth shall be encouraged in areas where it is possible to develop facilities incrementally.
- 7.14 To encourage groundwater recharge, ponding basins shall be designed as retention basins. However, pumping facilities shall be included in such facilities to handle peak flows and to provide for disposal of storm water into irrigation ditches when necessary. Stormwater inflow into Alta Irrigation District's canals and pipelines shall be subject to existing or future agreements by and between the City of Dinuba and Alta Irrigation District specifying maximum inflow, maximum service area boundary and any other limitation thereto.
- 7.15 In order to address sewerage constraints on the easterly side of the community, new developments shall demonstrate that adequate sewerage capacity exists prior to development or that mitigation measures will ensure that sewerage capacity will be created as part of the project. Mitigation measures may include installation of necessary facilities or other methods acceptable to the City.
- 7.16 New municipal water wells should be planned which include pump, storage, pressure filtration and treatment equipment. These new wells should be located so that they will not conflict with planned residential neighborhoods. New wells should have design, screening, landscaping and architectural improvements which make them compatible with adjacent land uses.

7.17 The City shall require industrial sewage pretreatment to conserve biological treatment capacity at the wastewater treatment plant. Water conservation measures should also be encouraged for industrial, commercial and residential uses to preserve hydraulic capacity at the treatment plant and to reduce impacts to the sewerage system.

7.18 The City should require the connection of existing and new businesses, residents and industries to the City's water and sewer system. The City shall establish equitable fees which enable it to recover the costs of such connection and improvement of any private or independent systems to City Standards.

## 7.2 HEALTH CARE FACILITIES

### OBJECTIVE

Facilitate a continued high level of health care services in the community.

### POLICY

7.19 The City of Dinuba will pursue the development of a new hospital or medical center (specialty clinics, assisted care, etc.) west of town off El Monte Avenue and Road 60 alignment. The new facility could be regional in nature and also support neighboring communities.

## 7.3 LOCAL GOVERNMENT FACILITIES AND SERVICES

### OBJECTIVE

Provide high quality government facilities and services to the general public. The location of government facilities and services shall be directed to the Central Business District of the community to the greatest extent possible.

### POLICIES AND STANDARDS

7.20 Maintain innovative solid waste service and programs.

7.21 Provide facilities according to the Public Safety Master Plan.

7.22 Ensure that the City's Capital Investment Plan is adequate to meet future growth and development needs in conformity with the goals, policies and objectives of the General Plan.

7.23 In conjunction with community parks, develop balanced services for each City quadrant with appropriate programs and services.

## 8.0 SAFETY ELEMENT

### INTRODUCTION

The quality of life in Dinuba is directly impacted by the sense of security of its residents and businesses. In order to provide a safe and enjoyable environment for residents, it is important to address the issues of crime, violence, and other human caused hazards, and to prepare a response to uncontrollable natural hazards. The Safety Element establishes objectives and policies and standards to ensure that there is an adequate, coordinated, and expedient response to public safety concerns.

### PURPOSE OF THE SAFETY ELEMENT

The purpose of the Safety Element is to identify and address those features or characteristics existing in or near Dinuba that represent a potential hazard to the community's citizens, sites, structures, public facilities, and infrastructure. The Safety Element establishes policies to minimize the danger to residents, workers, and visitors, while identifying actions needed to manage crisis situations such as earthquakes, fires, and floods. The Element also focuses on preventing criminal activity and violence before they occur. Additionally, the Safety Element contains specific policies to regulate existing and proposed development in hazard-prone areas.

### SCOPE AND CONTENT OF THE SAFETY ELEMENT

The Safety Element satisfies the requirements of state planning law and is a mandated component of the General Plan. Government Code §65302 (g) sets forth a list of hazards that the Element must cover, if they pertain to conditions in the City. These hazards are:

- Seismically induced conditions including ground shaking, surface rupture, ground failure, tsunami, and seiche;
- Slope instability leading to mudslides and landslides;
- Subsidence and other geologic hazards;
- Flooding;
- Wildland and urban fires; and
- Evacuation routes.

The Safety Element contains four sections: the Introduction; Purpose of the Safety Element; Scope and Content of the Safety Element; and Objectives, Policies and Standards. In the Objectives, Policies and Standards section,

major issues pertaining to hazardous conditions and safety are identified, and related policies established. The policies serve as guidelines for reducing the risks associated with humans, including criminal activity and natural hazards.

The policies also serve to direct and maximize community emergency preparedness.



### 8.1 STRUCTURAL SAFETY, TREATMENT PLANT, SANITARY SEWER, AND STORM DRAINAGE

#### OBJECTIVE

The following objectives and policies are excerpted from the Safety Element of the Tulare County General Plan. The objectives and policies have been adopted by the City of Dinuba and are incorporated in this General Plan Update. Additional policies and standards have been added where applicable.

A. Maintain the economic well being of structures and prevent structural damage.

#### POLICIES AND STANDARDS

- 8.1 The City of Dinuba will maintain an on-going active program designed to eliminate unfit, unhealthy, dangerous, structurally unsafe and fire hazardous housing units which are in such condition as to be reasonably beyond repair or rehabilitation. All departments or agencies having knowledge of such units or the vacancy of such units should notify the appropriate or concerned agencies.
- 8.2 The City of Dinuba will continue the program to have unsafe structures repaired or removed.
- 8.3 The City of Dinuba will give preference to those families that are displaced by code enforcement activities in the allocation of housing units that are produced by publicly assisted housing programs.
- 8.4 Encourage and assist families living in unsafe structures to find safer living units. It is policy to give priority, when possible, to locate those families in public housing programs.

*"Acceptable Risk" is defined as the level of risk that is deemed tolerable given expected consequences and benefits. Different levels of acceptable risk may be assigned to various hazards according to the nature of the proposed danger.*

*Acceptable levels of risk may range from "near zero" for nuclear plants and natural gas transmission line to "moderate" for farm structures and low-intensity warehouses.*



- 8.5 The policies regarding unsafe and unsanitary structures as contained herein shall apply even more importantly to structures that are used by the public, such as restaurants and theaters, and what is termed in this report “Critical Facilities”.
- 8.6 The City of Dinuba, through the land use planning process and Development Department programs, shall require structures such as nursing homes, housing for the elderly, and other housing for the mentally and physically infirm to locate within reasonable distance (less than one mile) from fire stations and other emergency service providers.
- 8.7 Develop better standards for numbering buildings on private driveways so as to assist emergency service personnel in locating structures in case of an emergency.
- 8.8 Encourage the installation of a system of heat and/or smoke detection devices and encourage a sprinkler system and other fire suppression equipment including fire hoses and water storage tanks or fire hydrants for all structures that exceed 5,000 square feet in floor area for the following facilities:
- Critical facilities (public buildings).
  - Permanent industrial facilities employing ten or more people on a year-round basis.
  - Housing for the elderly, children and mentally infirm.
  - Nursing homes and hospitals.
  - Structures where large amounts of chemicals or fuels are known to be stored and are considered to be significantly dangerous by the Fire Chief.
  - Any structure as required by the Fire Chief or other legislation.
- 8.9 The City of Dinuba will require a system of heat and/or smoke detection devices for the following facilities:
- Existing homes upon resale, and apartments every two years.
  - New homes to be constructed.
  - Structures with high value storage capacity.
  - Mobile homes.
  - Existing offices and other buildings.

- 8.10 Encourage fire alarm systems, as referred to in this Element, to be tied directly and automatically to the Tulare County Fire alarm receiving center. This would apply to private companies that wish to have better protection as well as public buildings and other structures where the Fire Chief and/or the Building Official deem it necessary to have such protection.



## 8.2 EDUCATION AND DISASTER PREPAREDNESS

### OBJECTIVE

Participate in regional education and disaster preparedness programs.

### POLICIES AND STANDARDS

- 8.11 Encourage fire and law enforcement departments to periodically conduct joint training exercises with the goal of developing the best possible coordinated action in fire suppression and crowd control.
- 8.12 The City of Dinuba shall maintain inventories of available resources to be used during disasters.
- 8.13 Continue to upgrade preparedness strategies and techniques at all levels of government in order to be prepared when natural or man-made disasters occur.
- 8.14 Work to reduce the possibilities of fire, flood and seismic disasters so that the objectives of the Insurance Services Office can be adequately met.
- 8.15 The City of Dinuba shall continue to coordinate a public education program in order to foster public awareness of fire hazards with the intention of reducing injury and loss of life, damage to property and degradation of the natural environment, particularly in conjunction with the public school system and “critical facility” personnel.
- 8.16 Carry out regular education programs through the public and private schools, the libraries, police and fire departments, news media, civic organizations, and through various related City departments.

- 8.17 Education programs conducted by the City of Dinuba shall seek to reach all age groups, socio-economic classes, and both urban and rural residents. Education programs should be offered in both Spanish and English languages as appropriate.

### 8.3 ENVIRONMENT

#### OBJECTIVE

Promote safety standards which maintain the physical environment.

#### POLICIES AND STANDARDS

- 8.18 The City of Dinuba has incorporated and will utilize the Multihazard Functional Plan (MFP) that adheres to all County, State and Federal guidelines. Incorporated within this Plan are guidelines for responding to emergencies related to:

- Major Earthquake
- Imminent/Actual Flooding
- Imminent/Actual Dam Failure
- Hazardous Material Incident
- War Emergencies
- Major Petroleum Shortages

Along with these guidelines, the City's administrative staff, department heads and City Council members have been trained in the MFP and the use of the Emergency Operation Center (EOC), which is located at the City Police Department's training room. These same personnel have also been trained in the basic use of the Incident Command System (ICS) as well as the Standardized Emergency Management System (SEMS).

- 8.19 Continue to locate and construct fuel breaks and greenbelts in appropriate locations on the periphery of the City in concurrence with other agencies.
- 8.20 The City of Dinuba shall require weed abatement programs in order to promote fire safety.
- 8.21 Assist in solving the incendiary problem by improving present law enforcement and investigation equipment; adapting equipment available in other fields; and purchasing new equipment where needed.

### 8.4 MANAGEMENT AND FUNDING

#### OBJECTIVES

- A. Maintain statistical information for safety issues.

- B. Establish responsibility and authority of agencies for emergency procedures.
- C. Provide cost-benefit analysis for participating agencies/jurisdictions.

#### POLICIES AND STANDARDS

- 8.22 The City will continue to collect and keep fire data in a form that combines the following:
- a. Number of fires by activity and area.
  - b. Number of users in the activity.
  - c. Number of fires by ignition index in State responsibility areas.
- 8.23 Damages and costs per fire should be computed and compiled by burn index and activity.
- 8.24 It is recommended that the City Fire Chief maintain statistical information in a form that can be geographically indexed for cost-benefit analysis by the City Council.
- 8.25 As part of the planning process, consideration shall be given to potential fire hazard. The Fire Chief may make recommendations regarding risk of hazard associated with the use of materials, types of structures, location of structures and subdivisions, road widths, location of fire hydrants, water supply and other important considerations regarding fire hazard that may be technically feasible but not included in present ordinances or policies.
- 8.26 The City of Dinuba will continue to implement the subdivision and zoning ordinances.
- 8.27 The City of Dinuba, in conjunction with the Tulare County Association of Governments, will participate in technical assistance programs.
- 8.28 The City of Dinuba will encourage the enlistment of the aid of courts, prosecuting attorneys, and the general public to make present laws more effective in dealing with the problems of illegal use of fire and fire causing practices.
- 8.29 The City of Dinuba will have a technically qualified communications officer to address the problem of communications within the County.
- 8.30 During major disasters, the primary coordinating official on behalf of the City shall be the City Manager.



## 8.5 PUBLIC SAFETY STANDARD

### OBJECTIVE

Adopt and implement safety standards for varying hazards.

### POLICIES AND STANDARDS

- 8.31 A 30 foot brush clearance zone around homes should be extended to greater distances where homes are situated on or near slopes (i.e. Smith Mountain). A formula should be adopted which relates percent of slope to width of brush clearance required and the formula should be included within the Zoning Ordinance.
- 8.32 Environmental Impact Reports shall be required on all projects in areas of extreme hazard as defined herein (a project is defined within the California Environmental Quality act).
- 8.33 Water supply systems shall be related to the size and configuration of land developments. Standards as set forth in the current subdivision ordinance shall be maintained and improved as necessary.
- 8.34 Development proposals shall take into consideration required fire standards, particularly in regard to critical facilities.
- 8.35 The City of Dinuba will enforce Chapter 70 of the Uniform Building Code as it relates to grading.
- 8.36 The City's Fire services response goal shall be five minutes from "tone-out" to arrival on scene.
- 8.37 Enforce policies and objectives of the 1974 Housing and Community Development Act, and all subsequent amendments, in order to insure safe and decent housing for low and moderate income families.
- 8.38 It is the policy of the City to maintain adequate street width and connectivity in the circulation system to enable prompt response and emergency access. Street widths shall conform to the State fire code which requires 20-feet of clear travel way on public roads or fire lanes. Development shall also conform with the following connectivity guidelines:
- Access to arterials, collectors or minor collectors should be provided at least every 500 feet.
  - Provide at least 250 street intersections per square mile to ensure a grid network of connectivity. Pedestrian cul-de-sac connections to public streets shall be counted as intersections if accessible by emergency vehicles.

*The floodplain is the relatively level land area on either side of the banks of a stream subject to flooding. The 80-year floodplain is the area subject to flooding based on a storm event that is expected to occur every 80 years on average, based on historical data.*

8.39 Neighborhood and local streets shall be designed for speeds of 25 miles per hour.

8.40 Traffic calming devices such as bulbouts, chokers, mid-block bulbs, traffic circles and textured sidewalks shall be encouraged, to keep speeds below 20 miles per hour.

## 8.6 FLOODING

### OBJECTIVE

Protect the lives and property of residents from the hazards of flooding.

### POLICIES AND STANDARDS

- 8.41 Consistent with Federal standards, the City shall implement FEMA regulations and design guidelines to address 80-year flood events, and require adequate storm drainage facilities (either retention or detention basins) to prevent flooding within the community.
- 8.42 All flood-proofing shall be done in a manner that will not cause floodwaters to be diverted onto adjacent property, increase flood hazards to property located elsewhere, or otherwise adversely affect other property.
- Flood-proofing measures such as, but not limited to, the following may be required:
- Anchorage to resist flotation and lateral movement.
  - Use of special water resistant paints, membranes, or mortars to reduce seepage of water through walls.
  - Addition of weight to structures to resist flotation.
  - Construction of water and waste systems to prevent the entrance of floodwaters.
  - Construction to resist rupture or collapse caused by water pressure or floating debris.
  - Location of all electrical equipment, circuits, and installed electrical appliances in a manner that will assure they are not subject to inundation by a 80-year flood.
  - Flood-proofing shall be required for structural storage facilities containing chemicals, explosives, buoyant materials, flammable liquids, or other toxic materials which could be hazardous to public health, safe-

- ty, and welfare. These shall be located in a manner which will assure that the facilities are (1) situated at elevations above the height associated with the 80-year flood protection elevation, or (2) adequately floodproofed to prevent flotation or storage containers or damage to storage containers which could result in the escape of toxic materials into floodwaters.
- 8.43 In flood-hazard areas, all public utilities and facilities, such as road, sewage disposal, gas, electrical, and water systems, shall be located and constructed to minimize or eliminate flood damage to the facilities. This work shall be done in a manner that will not adversely affect other property.
- 8.44 In flood-hazard areas, natural watercourses should be identified, and their flow capacities shall be preserved. This does not prohibit relocation. All grading, including relocation and agricultural grading, which can substantially affect natural drainage channels shall require a grading plan and City permit. The Director of Public Works shall review and approve the grading plan before work may be initiated.
- 8.45 Open space uses should be encouraged in all flood-hazard areas. Land Conservation Contracts and Open Space and Scenic Easements should be made available to property owners within 80-year flood areas.
- 8.46 The City should initiate a public awareness program to inform affected property owners of flood hazards on lands that can expect potential flooding.
- 8.47 The City's Zoning Ordinance shall be amended to reflect the mandatory policies expressed herein.



## 9.0 HOUSING ELEMENT

### GOALS, POLICIES, AND OBJECTIVES

Note: The City of Dinuba's Housing Element was recently adopted in 2004 and is in compliance with HCD. It has been reformatted only for the General Plan Update.

Housing goals, policies, and objectives serve as general policy guidelines for Dinuba. Consistent with state and national housing objectives, these goals, policies, and objectives reflect a commitment to provide a decent home and suitable living environment for every individual and family.

#### Goal 1:

Develop through public and private channels sufficient new housing to ensure the availability of affordable housing for all households in Dinuba.

#### POLICIES

1. Advocate and support proposed federal and state actions which will create a positive, stable climate for housing production.
2. Wherever appropriate, facilitate the use of federal or state programs which can assist in development of new housing consistent with identified City-wide housing needs and adopted local plans and programs.
3. Support efforts which serve to coordinate and improve the ability of the housing delivery system to effectively respond to local housing needs.
4. Accommodate and encourage development of a full range of housing types within the City.
5. Maintain a sufficient inventory of developable land to accommodate timely development of needed new housing supplies.
6. Encourage and participate in efforts designed to achieve economies and efficiencies which will facilitate the production of quality, affordable housing.
7. Promote balanced, orderly growth to minimize unnecessary developmental costs adding to the cost of housing.

#### OBJECTIVES

- A. Facilitate the construction of non-market-rate dwellings by 2008 which is the City of Dinuba's Regional Share.

- B. Facilitate the construction of market-rate dwellings by 2008 which is the City of Dinuba's Regional Share.

#### Goal 2:

Manage housing and community development in a manner which will promote the long-term integrity and value of each new housing unit and the environment in which it is located.

#### POLICIES

1. Provide that new housing be constructed in accordance with design standards that will ensure the safety and integrity of each housing unit.
2. Encourage application of community design standards.
3. Manage new residential development within the context of a planning framework designed to minimize adverse impacts on the area's natural resource base and overall living environment.
4. Manage neighborhood environmental factors such as traffic flow, school locations, parks, and open spaces and other public uses to stabilize and upgrade neighborhoods and dwellings.
5. Promote the use of "smart growth" techniques, including reduced front setbacks, narrow streets, limited on-street parking, and rear alley garages.

#### OBJECTIVES

- A. Maintain community design and improvement standards that will provide for the development of safe, attractive, and functional housing developments and residential environments.
- B. Maintain and update public service master plans, including water, sewer, and drainage.
- C. Adopt a Smart Growth Ordinance.

#### Goal 3:

Provide for a choice of housing locations for all residents.

#### POLICIES

1. Review and update the Dinuba General Plan on a regular basis to ensure that growth trends are accommodated.
2. Encourage the development of various types of housing opportunities in all residential areas.

3. Promote the use of the City's density bonus procedures, which encourage the provision of affordable housing.

**OBJECTIVE**

Maintain at least a 10-year supply of residentially-zoned land within the City Limits, ensuring that any future proposed development can be accommodated.

**Goal 4:**

Maintain and improve the quality of the existing neighborhoods and housing stock.

**POLICIES**

1. Monitor housing stock quality to maintain a current inventory of all sub-standard housing units.
2. Provide for removal of all unsafe, substandard dwellings which cannot be economically repaired.
3. Encourage development of sound new housing on vacant land within existing neighborhoods which have the necessary service infrastructure.
4. Support and encourage all public and private efforts to rehabilitate and improve the existing housing stock.
5. Promote public awareness of the need for housing and neighborhood conservation.
6. Manage public housing projects to ensure proper maintenance of the area's public housing inventory.
7. Support actions which foster and maintain high levels of owner-occupancy, particularly in those neighborhoods in which housing quality is declining.
8. Promote development of public policies and regulations which provide incentives for proper maintenance of owner-occupied and rental housing.
9. Manage development of land within and adjacent to existing neighborhoods to avoid potentially adverse impacts on the living environment.
10. Encourage proper maintenance of essential public services and facilities in residential developments.

11. Encourage available public and private housing rehabilitation assistance programs where such action is needed to ensure preservation of the living environment.

12. Facilitate maximum utilization of federal and state programs which can assist lower income homeowners to properly maintain their dwelling units.

**OBJECTIVES**

- A. Rehabilitate an annual average of ten dwellings for very low and low income households, through 2008.
- B. Conserve existing dwellings for very low and low-income households through 2008.
- C. Encourage the participation of Self-Help Enterprises, the Central Valley Coalition for Affordable Housing, and other non-profit developers to assist in the rehabilitation and conservation of housing within Dinuba.

**Goal 5:**

Promote equal access to safe and decent housing for all income groups.

**POLICIES**

1. Encourage enforcement of fair housing laws throughout the City.
2. Support programs which increase employment and economic opportunities.
3. Encourage development of a range of housing for all income levels in proximity to existing and planned employment centers.
4. Encourage full utilization of federal and state assistance programs which can enable those persons with unmet housing needs to obtain decent housing at process they can afford.
5. Support the development of housing plans and programs, including new government subsidized housing, which maximizes housing choice for minorities and lower-income households commensurate with need.
6. Wherever possible, implement adopted land development and resource management policies without imposing regulations which have the effect of excluding housing for lower income groups.
7. Develop record keeping methods to track the City's accomplishments in meeting its non-market-rate housing need allocation.



**OBJECTIVES**

- A. Facilitate the provision of homeowner and rental assistance to very-low and low-income households.
- B. Research and apply for housing assistance from applicable federal and state housing programs.

**Goal 6:**

Promote energy conservation in all residential neighborhoods.

**POLICIES**

1. Advocate and support proposed federal and state actions to promote energy conservation.
2. Promote public awareness of the need for energy conservation.
3. Promote development of public policies and regulations that achieve a high level of energy conservation in all new and rehabilitated housing units.
4. Encourage maximum utilization of federal, state, and utility company programs that assist homeowners in providing energy conservation measures.

**OBJECTIVE**

Maintain local ordinances to promote energy conservation.

**POLICIES**

1. Encourage enforcement of fair housing laws throughout the City.
2. Support programs which increase employment and economic opportunities.
3. Encourage development of a range of housing for all income levels in proximity to existing and planned employment centers.
4. Encourage full utilization of federal and state assistance programs which can enable those persons with unmet housing needs to obtain decent housing at process they can afford.
5. Support the development of housing plans and programs, including new government subsidized housing, which maximizes housing choice for minorities and lower-income households commensurate with need.

6. Wherever possible, implement adopted land development and resource management policies without imposing regulations which have the effect of excluding housing for lower income groups.
7. Develop record keeping methods to track the City's accomplishments in meeting its non-market-rate housing need allocation.

**OBJECTIVES**

- A. Facilitate the provision of homeowner and rental assistance to very-low and low-income households.
- B. Research and apply for housing assistance from applicable federal and state housing programs.